

CITY OF RENSSELAER ADA PUBLIC STREETS AND SIDEWALKS PLAN

Draft Report January 2025



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1.0 INTRODUCTION

1.1 Overview

The purpose of this ADA Self-Analysis and Transition Plan is to ensure that the City of Rensselaer pedestrian infrastructure in the public right-of-way is accessible for everyone, including people with disabilities. Federal standards for pedestrian facilities in the public right of way proposed in 2011 were finalized and became effective in September 2023, were adopted by the US Department of Transportation in December 2024, and will be fully enforceable once adopted by the US Department of Justice. The City has made a significant and long-term commitment to improving the accessibility of pedestrian facilities in the public-right-of-way.

Rensselaer has identified and evaluated all pedestrian infrastructure in the public right-of-way including curb ramps, sidewalks, crosswalks, pedestrian signals, and pedestrian accessible transit stops.

This document was funded by the Capital Region Transportation Council (Transportation Council), in part through a grant from the Federal Highway Administration, U.S. Department of Transportation. It is limited to pedestrian transportation facilities in the public right of way as well as shared use paths with a transportation function, and does not include any other programs, services, or activities such as buildings, parks, playgrounds, recreational trails, websites, or meeting locations. The views and opinions expressed herein do not necessarily state or reflect those of the U. S. Department of Transportation.

1.2 Mandate – Americans with Disabilities Act

The federal legislation, known as the Americans with Disabilities Act (ADA), enacted on July 26, 1990, provides comprehensive civil rights protections to persons with disabilities in the areas of employment, state and local government services, and access to public accommodations, transportation, and telecommunications.

1.3 Transition Plan

Title II of the ADA specifically applies to “public entities” (state and local governments) and the programs, services, and activities they deliver. Title II regulations require that each public agency develop a Transition Plan (also referred to as “Plan”) to bring any non-compliant programs, services, and activities into compliance.

The Plan shall, at a minimum:

- Identify physical obstacles in the public entity's facilities that limit the accessibility of its programs or activities to individuals with disabilities;
- Describe in detail the methods that will be used to make the facilities accessible;

- Specify the schedule for taking the steps necessary to achieve compliance with Title II and, if the time period of the transition plan is longer than one year, identify steps that will be taken during each year of the transition period;
- Indicate the official responsible for implementation of the plan.

Transition plans provide a method for a public entity to schedule and implement ADA required improvements to existing facilities. Before a transition plan can be developed, an inventory of the existing facilities must be developed.

Because the ADA is a civil rights law, the federal enforcement entity for the ADA is the Civil Rights Division of the US Department of Justice (DOJ). The DOJ enforces ADA through various means: establishing standards for meeting the requirements of the ADA; coordinating enforcement activities of other federal agencies; and, as necessary, taking legal action to require compliance with ADA by state and local governments.

The Access Board is a federal agency that promotes equality for people with disabilities through leadership in accessible design and the development of accessibility guidelines and standards. As a coordinating body, the scope of their recommendations and guidance includes but is not limited to public rights of way, public buildings, and medical facilities.

1.4 Public Right of Way Accessibility Guidelines (PROWAG)

The United State Access Board issued its final rule for accessibility guidelines for pedestrian facilities in public rights-of-way in September 2023. Not yet formally adopted by the US Department of Justice, the Public Rights of Way Accessibility Guidelines (PROWAG) were adopted by the US Department of Transportation in December 2024. They were utilized in evaluating pedestrian facilities in the preparation of this plan.

The PROWAG provide design guidance for a wide variety of facilities in the public right of way, including curb ramps, sidewalks, street crossings, transit stops, and signage. Many of the guidelines are particularly sensitive to people who use a wheelchair or have low vision, though the guidance is intended to eliminate all access barriers for people who have a disability.

1.5 Public Right of Way (ROW) and Jurisdiction

Defining jurisdictional boundaries or ownership of facilities within the public ROW and identifying the responsible entity is central to the successful implementation of the ADA Transition Plan. Three jurisdictional entities were identified in preparing this plan including the City, Rensselaer County, and the State of New York.

Although three jurisdictional entities possess rights-of-way within the City of Rensselaer, the legal responsibility of maintaining and improving non-conforming or deficient facilities does not necessarily fall to the owner of the right-of-way.

According to New York State Highway Law (Chapter 25, Article 12-B, §349-C), the State of

New York may install sidewalks. However, notwithstanding an agreement to the contrary, once installed, the City is obligated to maintain those sidewalks. The City currently transfers the responsibility of construction and maintenance of sidewalks to the property owner adjacent to the sidewalk per §129 “Maintenance of sidewalks and curbs” and § 130 “Construction and relaying of sidewalks and curbstones” of the City General Legislation. Under this provision, the City may notice a deficient condition and require action by the landowner.

The following table (Table 1) identifies the entity responsible for the maintenance and improvement of pedestrian facilities within the city. This table is representative only of jurisdictional entities at the City, County, or State level.

TABLE 1: RESPONSIBLE ENTITY FOR MAINTENANCE OF PEDESTRIAN FACILITIES

FACILITY TYPE	City of Rensselaer Right of Way	Rensselaer County Right of Way	New York State Right of Way
SIDEWALKS	City	City	City
CURB RAMPS	City	City	City
CROSSWALKS	City	County	New York State
PEDESTRIAN CROSSING SIGNALS	City	County	New York State

1.6 Installation of Conforming Facilities During Roadway Alteration Projects

In accordance with the *Department of Justice(DOJ)/Department of Transportation (DOT) Joint Technical Assistance on the Title II of Americans with Disabilities Act Requirements to Provide Curb Ramps when Streets, Roads, or Highways are Altered through Resurfacing* (<https://archive.ada.gov/doj-fhwa-ta.htm>) – when pedestrian walkways intersect a curb, and the adjacent roadway is being altered including by resurfacing, the entity facilitating the roadway alteration is also responsible for the provision of an accessible curb ramp. The same principle exists if an entity is undertaking a roadway alteration project that requires the removal of other pedestrian facilities, such as sidewalks. If the facility is removed, it must be reinstalled and conform to the most current accessibility standards. The DOJ document referenced above provides guidance on the definition of a roadway alteration, which includes: “reconstruction, rehabilitation, *resurfacing*, widening, and projects of similar scale and effect.”

1.7 Identified Obstacles in the Public Right-of-Way

The City of Rensselaer has conducted a detailed evaluation of obstacles to pedestrian infrastructure along all public rights-of-way within the City. The methodology and criteria are outlined in Appendix A. Sidewalks were evaluated for continuity, missing panels, heaving panels, physical obstructions, and minimum width. Curb ramps, crosswalks, transit stops, and pedestrian signals were evaluated against key PROWAG criteria. The results of the evaluation are presented in the following section(s).

2.0 DETAILED EVALUATION

2.1 Inventory

An inventory and assessment of all pedestrian facilities located along public rights of way within the City was completed by consultants provided by the Capital Region Transportation Council.

Initially, a sidewalk inventory (including related pedestrian features and transit stops) was provided by the Transportation Council and this information was updated based on a review of the most current aerial photography and Google Street View. This information was used as a base map for additional data collection and condition assessment. New sidewalks may have been constructed during or shortly before the completion of this document. Over time, changes to the pedestrian infrastructure should be incorporated into the inventory and monitored for continued compliance.

Field data collection was accomplished on a mobile device (a Tablet, iPad, Trimble, or mobile phone) and the ESRI applications ArcGIS Field Maps and Survey 123. The consultant’s staff were trained in the evaluation and scoring of pedestrian features for compliance and subsequently completed the inventory and collected photographs and other important site-specific information.

Pedestrian features evaluated included sidewalks, transit stops, and key intersection features including curb ramps, pedestrian signals, and crosswalks. Evaluation criteria focused on compliance with key elements adapted from the PROWAG. The evaluation methods are included in Appendix A.



Photos included throughout this report are examples for demonstration purposes and are not representative of conditions in Rensselaer.

To complete the detailed evaluation, an accessibility condition rating for each of the identified facilities was determined. The City of Rensselaer used the following ratings and definitions to assess the condition of each sidewalk facility on local, county, and state rights-of-way.

TABLE 2: CHARACTERISTICS OF ACCESSIBILITY CONDITION FOR SIDEWALKS

	CHARACTERISTICS	EXAMPLE
1. FULLY ACCESSIBLE	<ul style="list-style-type: none"> Designed to Meet Current Standards 	<ul style="list-style-type: none"> Newly Installed Facility
2. MOSTLY ACCESSIBLE	<ul style="list-style-type: none"> Provides Access Not Fully Compliant 	<ul style="list-style-type: none"> Curb Ramps <i>Without</i> Detectable Warnings Limited Minor Defects
3. PARTIALLY ACCESSIBLE	<ul style="list-style-type: none"> Limited Accessibility 	<ul style="list-style-type: none"> Several Minor Defects
4. NOT ACCESSIBLE	<ul style="list-style-type: none"> Significant Discontinuity Inaccessible 	<ul style="list-style-type: none"> No Curb Ramps Steps Significant Heaving

Note: Additional information on evaluation criteria, including example imagery of sidewalk conditions, can be found in Appendix A.

To evaluate the accessibility of more complex multi-part features like curb ramps, crosswalks, and transit stops a rating matrix was used. Depending on the severity and number of deficiencies at any one of these features, they were assigned a pass/fail rating.

2.2 Sidewalks

The City of Rensselaer has a total of 42.38 miles of sidewalks that have been assessed for compliance with PROWAG. Sidewalks are assessed for missing panels, heaving panels, obstructions, and panel sidewalk width. Sidewalks are rated at the segment level, reflecting the usability by a person walking along the sidewalk. Typically, one sidewalk segment equates to one city block unless external circumstances – such as a driveway for a significant commercial activity like a grocery store – causes a significant break in the sidewalk on that block. Approximately 13.6 percent of segments in the city are rated Fully Accessible, and about 6.7 percent of segments are rated not accessible, the least common rating. Approximately 50 percent of segments in the city are rated Mostly Accessible. In total, approximately 34 percent of all segments have a rating that requires repair or improvement (Partially Accessible or Not Accessible).

It should be noted that because field evaluation ratings are conducted at the segment level, short segments are less likely to have conditions that would impact their rating while the

opposite is true for long blocks. Knowing this, the city should use this evaluation as a tool to identify segments that have barriers that must be remedied and focus remediation efforts on specific barriers rather than entire block segments.

TABLE 3: BLOCK LEVEL SIDEWALK SCORE RESULTS

ACCESSIBILITY RATING	PERCENTAGE (%) OF TOTAL MILES	LENGTH (MILES)	NUMBER OF SEGMENTS	PERCENTAGE (%) OF SEGMENTS
FULLY ACCESSIBLE	11.37%	4.82	96	13.64%
MOSTLY ACCESSIBLE	53.4%	22.63	353	50.14%
PARTIALLY ACCESSIBLE	28.52%	12.09	191	27.13%
NOT ACCESSIBLE	5.99%	2.54	47	6.68%
GRAND TOTAL	100%	42.38	704	100%

The most common condition that creates barriers to accessibility are obstructions and heaving panels, which were present on approximately 20 percent of all block segments assessed. Obstructions, in terms of this report, are considered fixed obstructions including but not limited to parking signs, utility poles, or severe overgrowth interrupting the access route. The presence of heaving panels or obstructions may not alone result in a Not Accessible or Partially Accessible rating depending on the severity and frequency of heaving or type of obstruction. Sidewalk segments rated Mostly Accessible should be monitored to ensure these segments remain accessible and conditions do not worsen over time.

A detailed table depicting the City’s inventory of sidewalks is included in Appendix D.

TABLE 4: BLOCK LEVEL SIDEWALK BARRIER CHARACTERISTICS

BARRIER CHARACTERISTIC	NUMBER OF BLOCKS WITH BARRIER	PERCENT (%) OF ALL BLOCKS
Missing Panel	41	5.78%
Heaving Panel	141	19.89%

Obstruction	128	18.05%
Too Narrow	59	8.32%

2.3 Curb Ramps

All intersections where sidewalks intersect roadways were observed for the presence of curb ramps, whether the curb ramp was flush with the roadway and whether a detectable warning surface (DWS) was present. 639 intersection access points were evaluated to assess their condition and accessibility.

47.8 percent of the observed curb ramps have a condition that may impact their accessibility, the most frequent of which are related to detectable warning surfaces. Detectable warning surfaces (DWS) are used to indicate to the visually impaired that they have reached an intersection with a roadway. The absence of DWS was a contributing factor to accessibility barriers in 37.5 percent of all curb ramps. The most common conditions identified with DWS that may impact accessibility are insufficient contrast of the DWS with the sidewalk, followed by deterioration or damage of the DWS. Finally, 6.4 percent of curb ramps are not flush with the asphalt where they meet the road, which can create problems for people with mobility issues.

The PROWAG Guidelines determine a minimum contrast of a light-on-dark or a dark-on-light to be considered accessible.

TABLE 5: CURB RAMP CONDITIONS

Type of Barrier	Count	Percentage
No Curb Ramp Present	59	9.1%
Curb Ramps without Barriers	303	52.2% (of those with curb ramps)
Curb Ramps with Barriers*	277	47.8% (of those with curb ramps)
DWS with Barriers*	58	13.8% (of those with DWS)

*Curb ramps and DWS surfaces may have multiple barriers

TABLE 6: CURB RAMP BARRIERS

Curb Ramp Barriers	Count	Percentage of those with
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		curb ramps
Not Flush with Asphalt	37	6.4%
Inadequate Width	12	2.1%
DWS Absent	217	37.5%
DWS with Barriers*	58	13.8% (of those with DWS)

*Curb ramps and DWS surfaces may have multiple barriers

TABLE 7: DWS BARRIERS

Detectable Warning Surface Barriers	Count	Percentage of those with DWS
Insufficient Width	8	2.0 %
Insufficient Depth	7	1.7%
Insufficient Contrast	42	10.0%
Deterioration/Damage	31	7.6%

In addition to conditions that may create barriers to accessibility, curb ramps and DWS were inventoried for characteristics that may assist the City in determining repair costs, replacement methods, and standard practices. Most of the City's curb ramps serve one direction of travel and most of the City's DWS are truncated domes.

TABLE 8: CURB RAMP INVENTORY CHARACTERISTICS

CURB RAMP CHARACTERISTIC	COUNT	PERCENTAGE OF CURB RAMPS W/DWS
Ramp Serves One Directions of Travel	354	84.1%
Ramp Serves Two Directions of Travel	66	15.6%

TABLE 9: DWS INVENTORY CHARACTERISTICS

DWS INVENTORY CHARACTERISTICS	COUNT	PERCENTAGE OF DWS
DWS Truncated Domes	387	91.9%
DWS Pavement Grooves	33	7.8%

2.4 Pedestrian Signals

Pedestrian signals let motorists know that pedestrians have the right of way, and let pedestrians know when it is their turn to cross a roadway. These signals were evaluated for the presence of a push button to request the crossing phase and/or crossing information, the crossing signal method (audio, visual, or both), and wheelchair accessibility (height and distance of the push button from the access route). The most common condition that may create barriers for pedestrian signal accessibility is that most signals have only visual cues indicating safe times to cross. 38.5 percent of the City’s pedestrian signals are visual only. Of the inaccessible pedestrian signals, five fall along State Roads and are therefore features under NYS Department of Transportation jurisdiction and maintenance.

Accessible pedestrian signals (APS) are defined in PROWAG as integrated devices that communicate information about the WALK and DON’T WALK intervals at signalized intersections in non-visual formats (i.e., audible tones and vibrotactile surfaces) to pedestrians who are blind or have low vision. It is recommended that the City adopt an APS policy to guide implementation of new APS as well as upgrades of existing traffic signals with or without pedestrian signals to include APS installation.

Signals with audible tones at each signalized intersection were also assessed qualitatively for their volume and their volume adjustment for surrounding ambient noise. The locator tones indicating to an individual to walk, or not walk, and indicating which road was being crossed, did not change in volume according to ambient noise. However, the continuous audio beacons which sound during the duration of the crossings did increase in volume based on surrounding ambient noise.

TABLE 10 PEDESTRIAN SIGNAL CONDITIONS

Type of Barrier	Count	Percentage of pedestrian signals
Visual or Audible Only	35	38.5%
Visual Only	35	38.5%

Audible Only	0	0%
Both Visual and Audible	55	61.1%
Not Wheelchair Accessible	0	0%

2.5 Pedestrian Crossings

Where sidewalks exist, intersections were observed for the presence of a pedestrian access route across the roadway, the condition of the access route, and the condition of marked crossings. All roads and intersections were observed for the presence of a pedestrian access route at both ends of the crossing, the condition of the crossing, whether there is a painted crosswalk, and the condition of the crosswalk paint. The condition and accessibility of 321 crossings was collected. Nearly 95 percent have a complete access route. Fair or poor route condition was a common barrier for crossings with a complete access route. The most common conditions are caused by potholes or significant cracking in the roadway where the pedestrian crossing exists. Of the inaccessible crosswalks identified, one falls along a State Road and is therefore a feature under NYS Department of Transportation (DOT) jurisdiction and maintenance. Additionally, poor condition of crossing markings was identified at 20.7 percent of all marked crossings. While crosswalk markings are not required under PROWAG, the Manual on Uniform Traffic Control Devices (MUTCD) requires solid white lines on crossings that are marked. Marked crossings that contrast with the roadway may also provide a visual indicator of the pedestrian access route for the visually impaired. There was one barrier at the time of data collection that fell into the “other” category. This was due to a parked vehicle being in the way of the crossing at the time of the collection. Because this is not a permanent constraint on the crossing accessibility, it is not included in any cost estimates for repair.

TABLE 11: PEDESTRIAN CROSSING CONDITIONS

Type of Barrier	Count	Percentage of all Pedestrian Crossings
Complete Access Route	304	94.7%
Incomplete Access Route	17	5.3%
Fair or Poor Route Condition	24	7.5%
Cracks/potholes	19	79.2%
Improper Cross-slope	1	4.2%

Other	1	4.2%
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Note: curb cuts and crosswalks are evaluated separately. There is no relationship between the number of crossings and curb cuts evaluated due to variability of curb cut presence at crossings.

TABLE 12: PEDESTRIAN CROSSING BARRIERS

Poor Route Condition Barriers	Count	Percentage of Pedestrian Crossings with Fair or Poor Route Condition
Cracks/potholes	19	79.2%
Improper Cross-slope	1	4.2%
Other	1	4.2%

TABLE 13: CROSSWALK MARKING CONDITIONS

Condition	Count	Percentage
Fair or Poor Crosswalk Condition	28	20.7% (of those with marked crossings)

2.6 Transit Stops

The Capital District Transportation Authority (CDTA) provides transit service to the City. 58 transit stops were assessed for accessibility. Transit stops within 50ft of sidewalks were assessed based on their connectivity to existing sidewalks and the conditions of their boarding and alighting area. A boarding and alighting area is defined in PROWAG as a level and stable surface for boarding vehicles. 55.2% of all stops have boarding and alighting areas with a condition that may create barriers to accessibility. The most common conditions are insufficient depth of the boarding and alighting areas and non-accessible surface materials.

TABLE 14: TRANSIT STOP CONDITIONS

Type of Barrier	Count	Percentage of all Stops
Boarding and Alighting Areas with Barrier*	32	55.2%
Insufficient Width	1	1.7%
Insufficient Depth	30	51.7%

Non-Accessible Surface Material**	29	50.0%
Obstructed Access Route	1	1.7%

**Note: Boarding and Alighting Areas may have multiple barriers identified*

***Note: Grass or dirt surface*

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FIGURE 1: SIDEWALK ADA ASSESSMENT

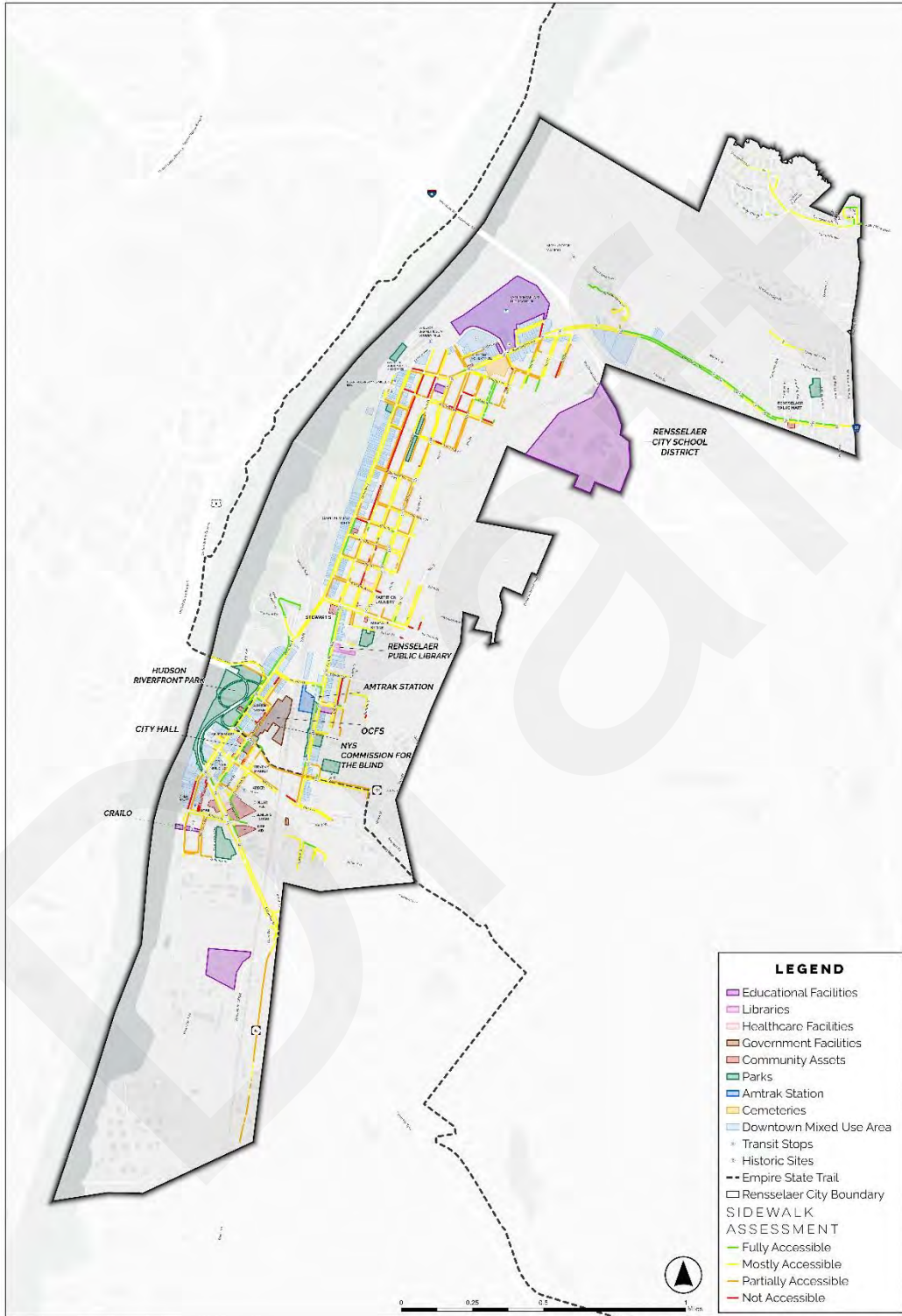


FIGURE 2: ADA INTERSECTION ASSESSMENT

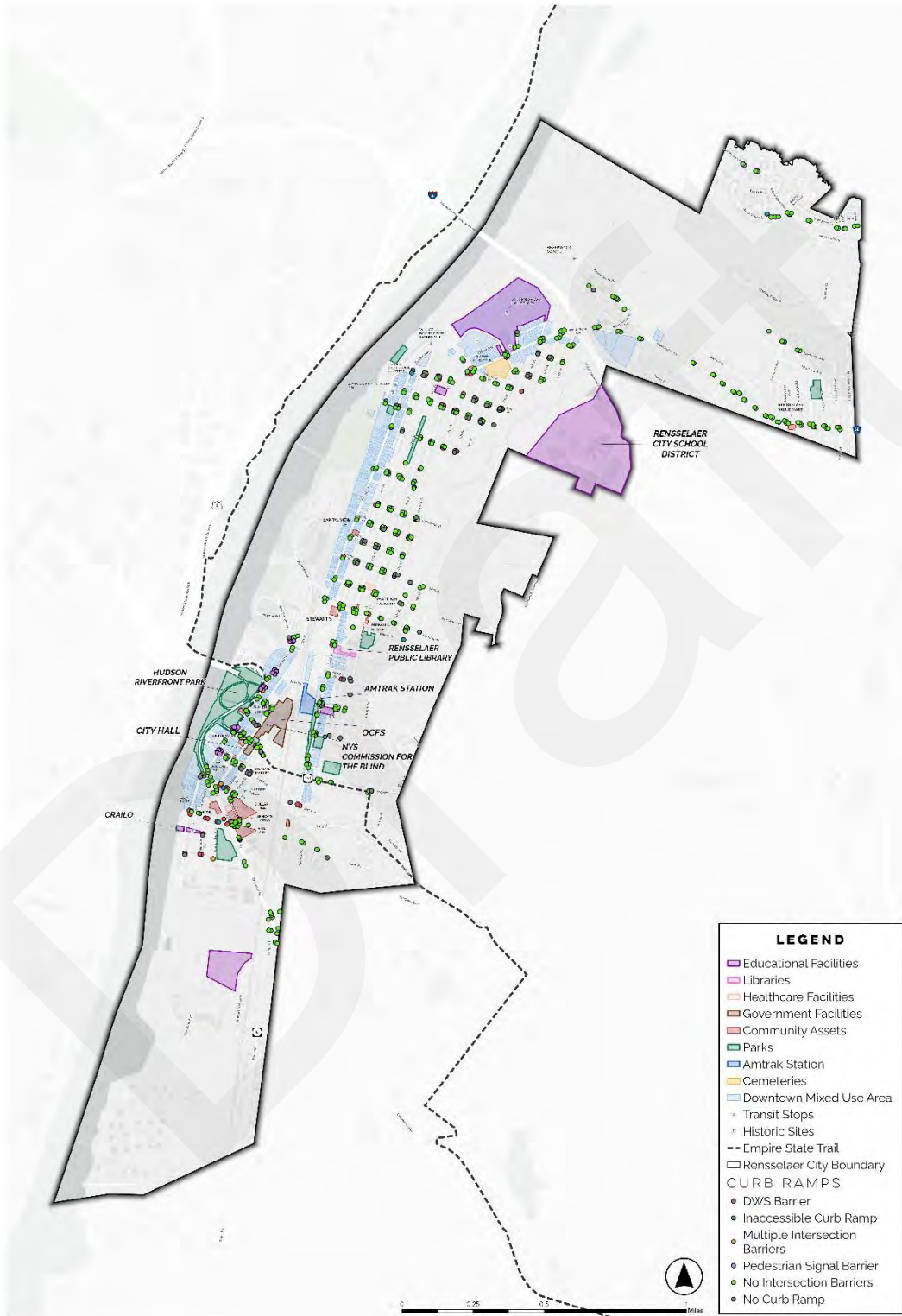


FIGURE 3: ADA CROSSING ASSESSMENT

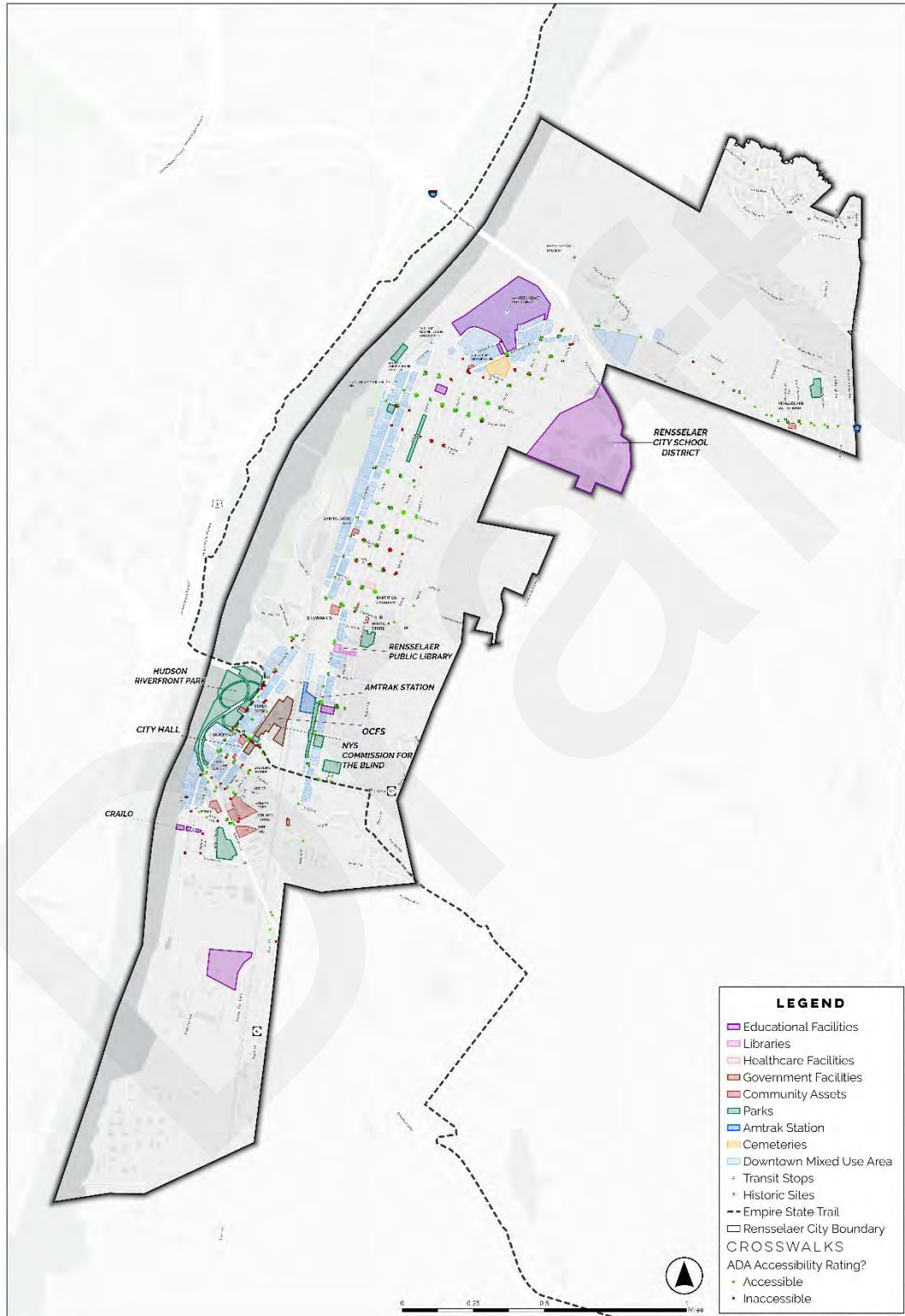
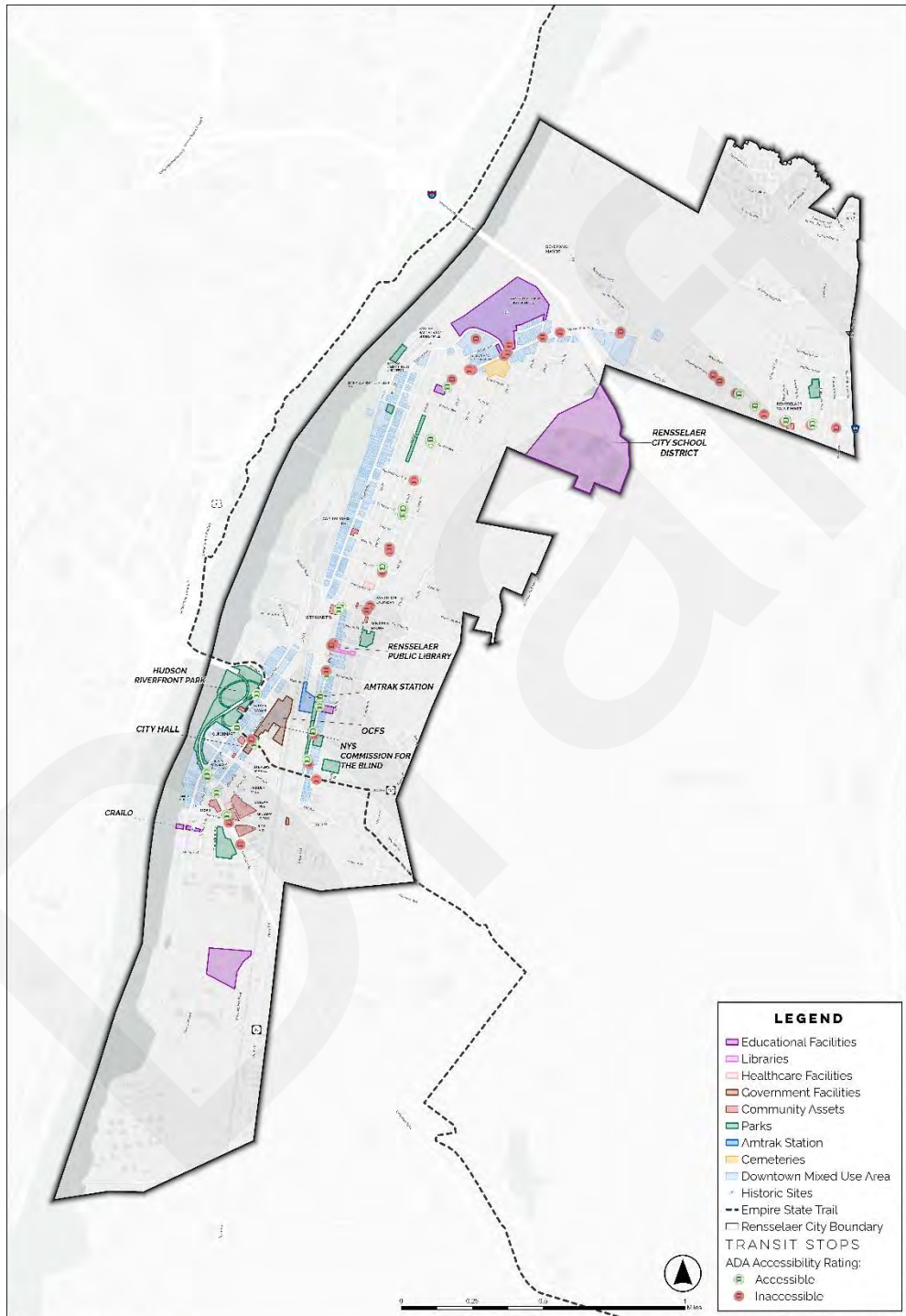


FIGURE 4: ADA TRANSIT STOP ASSESSMENT



3.0 METHODS TO REMOVING BARRIERS – POLICIES AND PRIORITIES

The City of Rensselaer plans to utilize several approaches to removing barriers in the public right-of-way, including notifying a landowner of a deficient condition and requiring action, responding to public complaints, pursuing funding to complete municipal projects themselves, and ensuring the appropriate design and build-out of new construction following the most recent design guidelines. The City of Rensselaer plans to base barrier removal on several priorities, specifically focused on the location of and accessibility of each facility. Additionally, the City may modify this schedule to respond to public complaints and to take advantage of public and private improvements.

3.1 Location Priority

According to the ADA Title II regulations, the transition plan shall give “priority to walkways serving entities covered by the Act, including State and local government offices and facilities, transportation, places of public accommodation, and employers, followed by walkways serving other areas” (§ 35.150 Existing Facilities (d)(2)). The City of Rensselaer will also prioritize land uses downtown, residential areas, and major roadways. During the repair process, the City intends, when possible, to pay particular attention to areas surrounding “Community Assets” defined as locations that provide access to food, grocery, laundry, and other vital services. They also intend while addressing inaccessibility in residential neighborhoods, when possible, to especially focus in areas where housing designed for persons with disability or affordable housing is present. The following prioritization methodology is intended to provide the City with a framework to schedule repairs and does not prevent them from making other repairs as the opportunity occurs.

Following Title II regulations, and specific considerations of the City, the City of Rensselaer identified its location priority as follows, beginning with the highest priority:

Priority 1. Government Facilities

This priority includes both City and State government offices, schools, parks and libraries, transit stops, and the Amtrak station. Many of these locations, including the Office of Children and Family Services campus, the City School District, the Amtrak Facility, and City Hall, also double as many of the City’s largest employers so is a vital priority for the City.

Priority 2. Commercial Corridors & Community Assets

This priority includes the commercial downtown area of Rensselaer, as well as “Community Assets”, as defined above, places of worship, and healthcare facilities.

Priority 3. Residential Neighborhoods

This priority includes all residential neighborhoods.

Priority 4. Major Roadways

This priority includes Columbia Turnpike, Broadway, Washington Avenue, and East Street in the City. Columbia Turnpike

Priority 5: All Others

Figures 5 and 6 illustrate the priority sidewalk segments regardless of jurisdiction. Full-size figures are presented in Appendix B.

Figure 5 reflects the prioritization method only for facilities identified through data analysis as “Not Accessible” or “Partially Inaccessible.” Alternatively, Figure 6 identifies all existing sidewalk facilities by the prioritization method, regardless of their current accessibility.

FIGURE 5: ADA PRIORITIZATION MAP BY EXISTING CONDITION

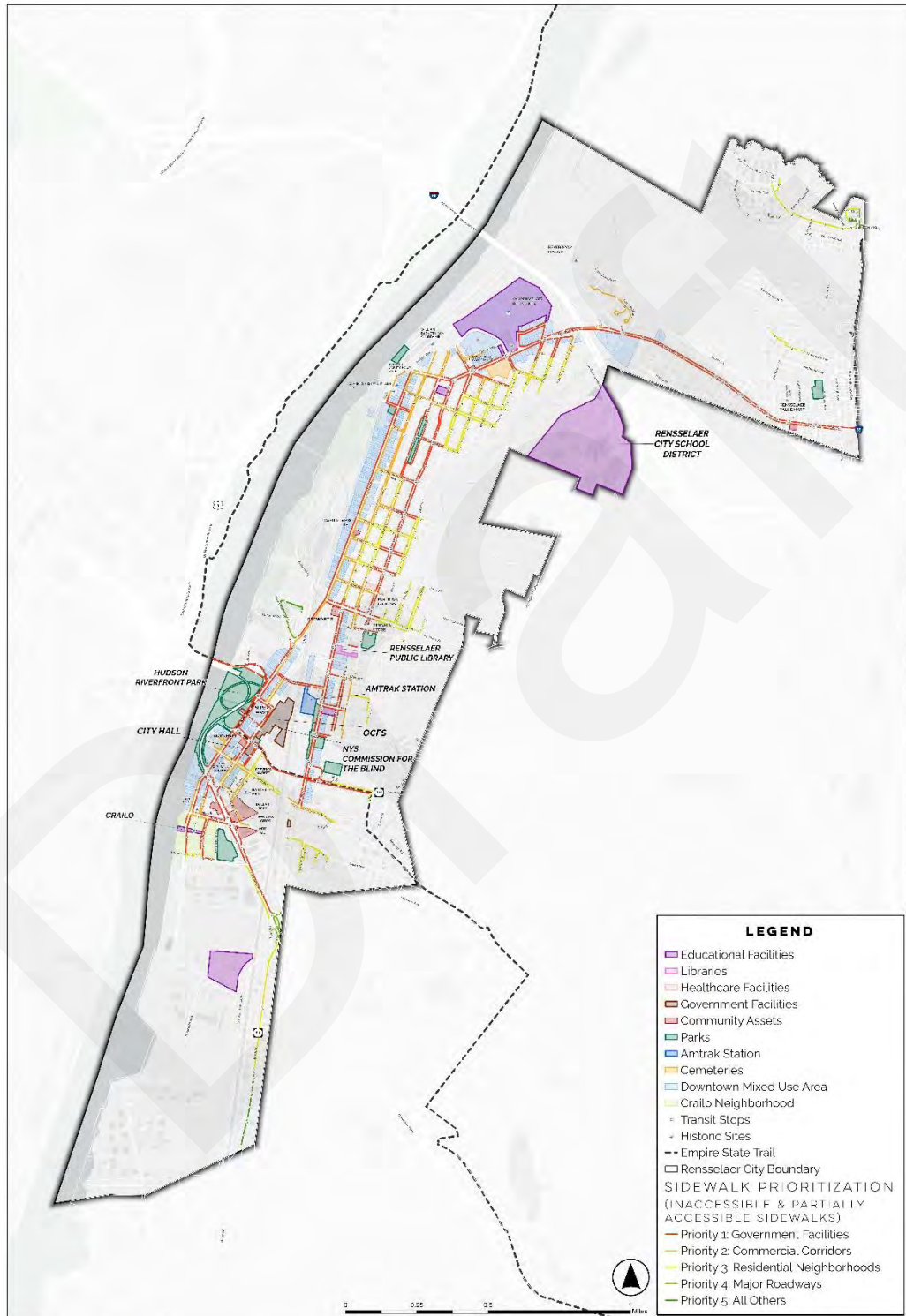
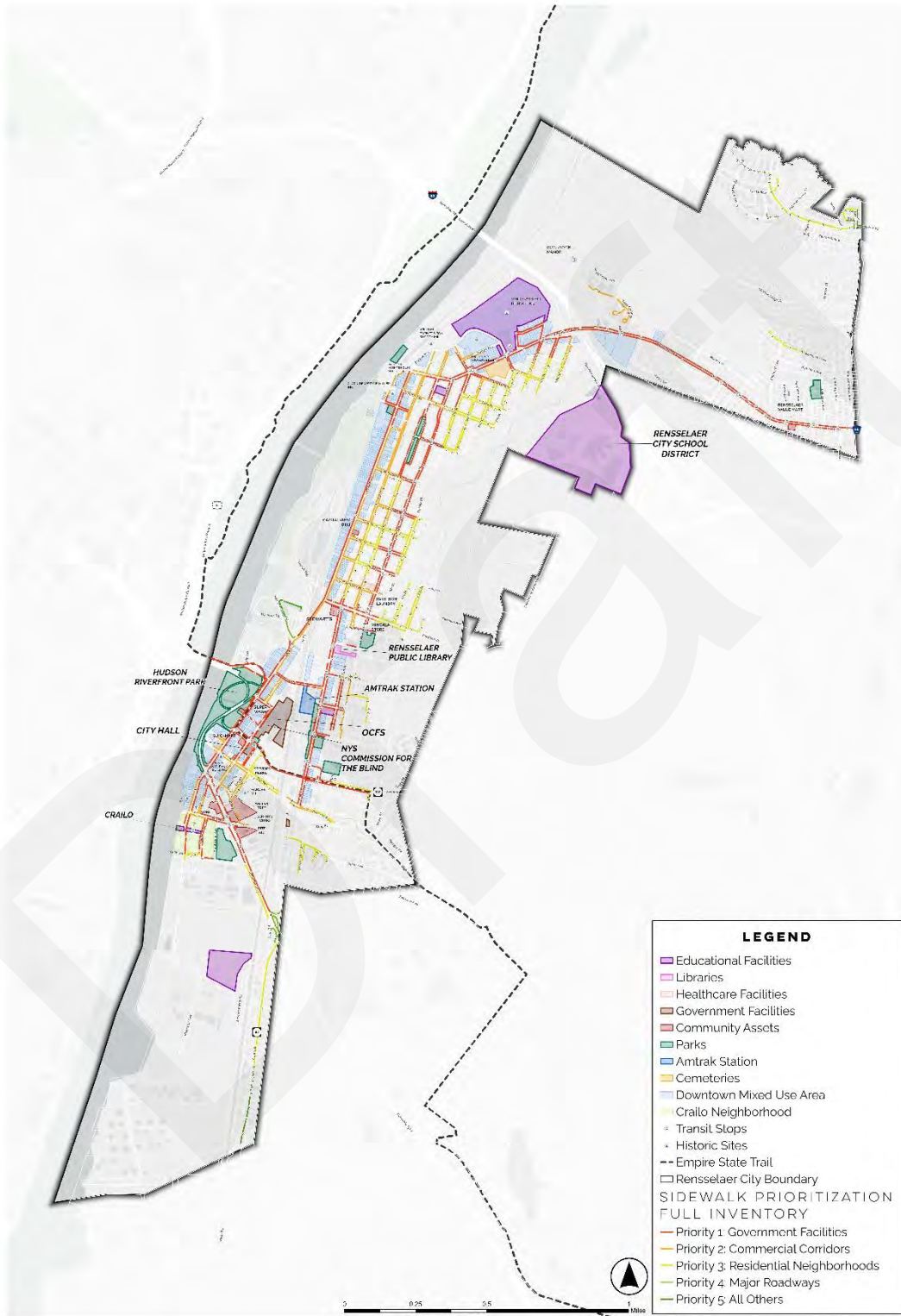


FIGURE 6: GENERAL ADA PRIORITIZATION MAP



3.2 Public Complaint Process

The public complaint process is an integral part of this plan. Public complaints or requests may drive the prioritization of improvements. To file a complaint or a request regarding accessibility of a sidewalk or curb ramp, submit a formal grievance in writing to the City's ADA Coordinator, the Assistant Planning Director, with the location, date, and description of the problem. The ADA Coordinator, within 60 days of the filing of the complaint, will conduct and complete an investigation, including, if necessary, a meeting with the complainant to discuss the complaint and possible resolutions. No later than 60 days after submittal a determination shall be issued by the ADA Coordinator in writing, and where appropriate, in a format accessible to the complainant, such as large print, Braille, or audio tape. The determination will explain the position of the City of Rensselaer and offer options for substantive resolution of the Complaint.

Appendix F is a copy of the City's public Grievance Procedure under the Americans with Disabilities Act.

3.3 New Construction and Alterations

To ensure the correct design of curb ramps, sidewalks, and crosswalks in new construction and alterations, the City of Rensselaer, through the adoption of this Transition Plan, has adopted the Public Right of Way Accessibility Guidelines (PROWAG) as finalized in 2023, (herein after known as ADA Guidelines) or the most current regulatory guidelines in the event PROWAG becomes outdated. Once adopted by the US Departments of Justice and Transportation, PROWAG will be the primary regulatory standards that govern the design and construction of all pedestrian facilities. It is important to note that in alteration projects, whenever pedestrian facilities cannot fully meet the standards outlined in the ADA Guidelines, a determination of each structural implacability or technical infeasibility must be thoroughly documented in the permanent project record.

The City of Rensselaer's commitment to ensuring the accessibility of facilities in the public right of way is clearly demonstrated by the Work Plan (Appendix D) and schedule outlined in this report.

As previously discussed under the Jurisdiction section of this report, if another jurisdiction is undertaking alterations of a roadway, including repaving, that agency is responsible for the replacement of removed pedestrian facilities or for the construction or rehabilitation of a curb ramp to the adjacent sidewalk if an accessible curb ramp does not exist.

As described in NYSDOT's current Transition Plan, NYSDOT undertakes on-going road maintenance and alterations to ensure accessibility of pedestrian facilities on State owned roads. For the purposes of this Transition Plan, areas lacking the provision of curb ramps in State owned rights-of-way will be identified in this plan as being undertaken by NYSDOT. The improvement of these facilities will correspond to the approved NYSDOT capital improvement program.

Nothing contained in this work plan would in any way prevent the City of Rensselaer from

modifying the implementation schedule.

3.4 Cost and Schedule

The detailed evaluation of the City of Rensselaer's sidewalk network identified a total of 14.63 miles of sidewalk segments rated Partially Accessible (12.09 miles) or Not Accessible (2.54 miles) which will require repair or replacement of barriers to bring them to compliance. For the purpose of estimating construction costs, it was assumed that 30% of the Not Accessible segment panels will require replacement and 15% of the Partially Accessible segment panels will require replacement.

The cost of the repairs and replacement was estimated utilizing the NYSDOT Quick Estimator Reference. This estimate is provided as Appendix C and is summarized as follows: Utilizing the scoring of facilities provided, sidewalk replacement totals \$935,956, Curb Ramps \$604,800, Pedestrian Signals \$139,400, and Crosswalks \$32,200. It should be noted that the actual costs will vary depending on the extent of repairs, actual site conditions and the date of construction.

Utilizing the prioritization method detailed above, the City will provide notice of needs of sidewalk repair, starting with highest priority areas, to adjacent landowners and provide them a deadline to have the work performed. If the landowner fails to make the required remedy, the City may choose to complete the work.

The City of Rensselaer has set a goal of bringing 2.93 miles of these segments to compliance each year, which will bring the City to compliance in approximately 5 years, contingent on the availability of funding. As identified during the location prioritization process, pedestrian facilities in the public right of way adjacent to government facilities, commercial areas, residential areas, and major roadways will be prioritized before facilities serving other areas. Curb ramps, crossings, and pedestrian signals will be addressed with their corresponding sidewalk segments or during road paving and resurfacing projects.

As opportunity allows, the City will make efforts to collaborate with stakeholders to improve the ADA accessibility of pedestrian facilities in the public right-of-way. Compliance is required to the extent feasible within the scope of the project. There will be times when it is technically infeasible to provide full compliance with the guidelines for example, if clear space at the top of the ramp is obstructed by a building or the slope of a hill is so extreme as to prevent a reasonable slope for a ramp in both directions. The inventory process may not account for such situations and could show a high-priority rating when all practical actions have been taken.

4.0 MONITORING AND UPDATES

4.1 Transition Plan Management

As with any effective planning document, the City of Rensselaer's Transition Plan will require continued review and updating. It is recommended that the City review and update this document on a five-year cycle. However, this does not preclude the City from altering that five-year cycle. If new regulations are adopted or new information is available that does not alter the intent of the transition plan, the City intends to add appendices to the document reflecting those changes. Members of the public that wish to see changes made to the plan are encouraged to submit their comments and concerns, in writing, to the ADA Coordinator to consider including during the next update.

In addition, the City will document progress made annually, for public review and comment. Members of the public that wish to see changes made to the plan are encouraged to submit their comments and concerns, in writing, to the ADA Coordinator to consider including during the next update.

4.2 Coordination with Public Improvements and Capital Expansion

The City has identified a schedule for improvement to non-compliant facilities (Appendix D). However, this schedule may be modified to respond to complaints and to take advantage of regular capital improvement projects or private development to upgrade deficient facilities, if occurring adjacent to those facilities.

The City will coordinate with the New York State Department of Transportation (NYSDOT) capital improvement schedule to replace or upgrade non-compliant curb ramps and sidewalks along State highways. Likewise, the City will coordinate with Rensselaer County to replace or upgrade non-compliant curb ramps and sidewalks along County roads.

5.0 PUBLIC INPUT

The City of Rensselaer provided opportunities for individuals to comment and provide input on this Transition Plan, which includes:

- Early results of the field assessment and prioritization framework were presented to the City's Advisory Committee for feedback.
- The plan will be presented to the City of Rensselaer City Council, and council members will be encouraged to provide feedback on behalf of their constituents.
- Copies of the draft Plan will be made available for public review and comment at City Hall, and the library.
- The ADA Transition Plan has also been made available on the City's website at <https://rensselaerny.gov/> and posted on the City's social media.

- The City of Rensselaer will open a public comment period. Members of the public will be encouraged to provide feedback by calling and/or emailing City staff to log their comments.

5.1 Comments Received

This section will be completed at the conclusion of the public comment period.

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APPENDIX A: EVALUATION CRITERIA

Photos included throughout this report are examples for demonstration purposes and are not representative of conditions in Rensselaer.

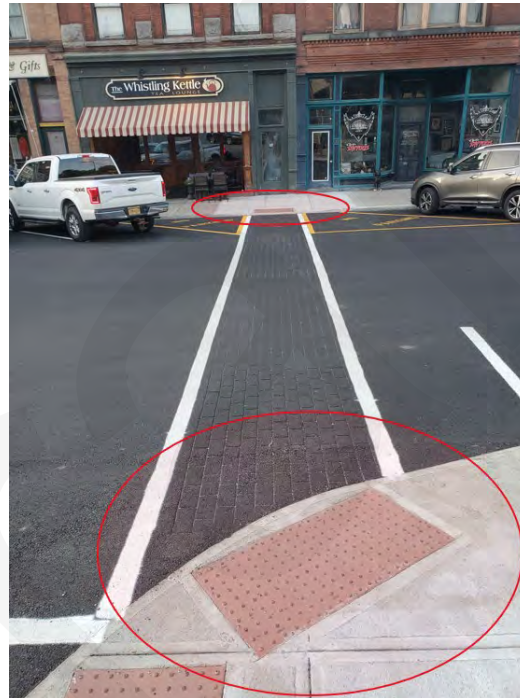
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APPENDIX A.2: PAR AT CROSSINGS

When crossing an intersection, the pathway does NOT need to have a crosswalk visibly marked. It only requires a curb ramp on both sides of the street. If there is a cross walk, it should be maintained as explained in Appendix B.1 and B.2. When crossing over train tracks, the gap between the rails and crossing surface should be no wider than 2.5in. for commercial trains, and 3in for freight trains.

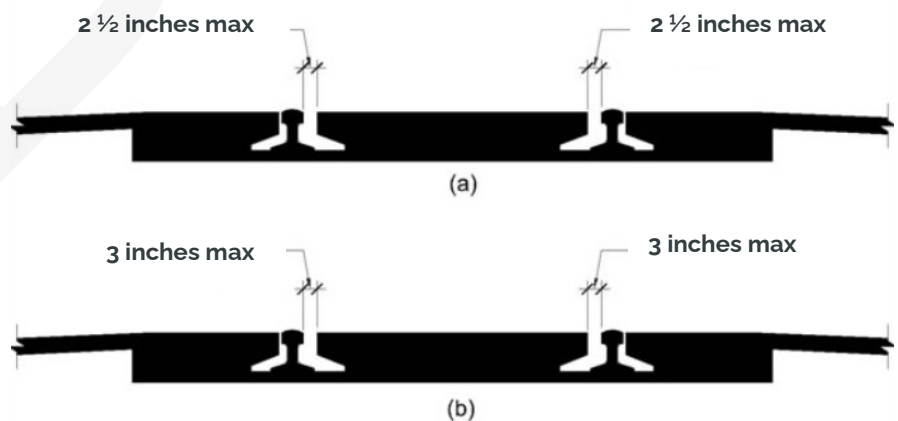
Street Crossings:

While often marked, it is not required. The location of a curb ramp on both sides' makes a complete pedestrian access route. If there is a curb ramp on one side and not the other, then the path is considered inaccessible.



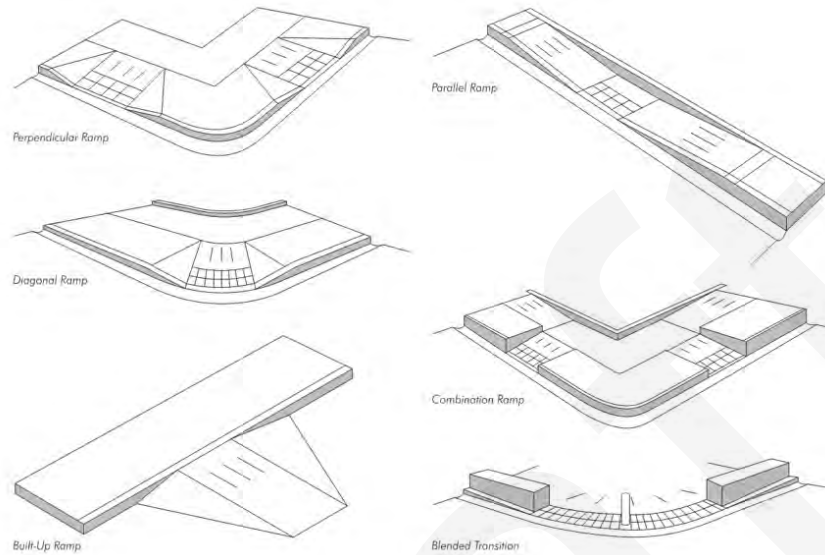
Railroad Crossing:

When crossing over train tracks, the gaps are left for the train to be able to roll through. The tracks are to be at the same height as the crossing made available.



APPENDIX A.3: CURB RAMPS

There are many different types of curb ramps that can be used for different types of intersections.



Additionally, curb ramp options shown below can be incorporated into a design as an alternate approach to traditional curb ramps.

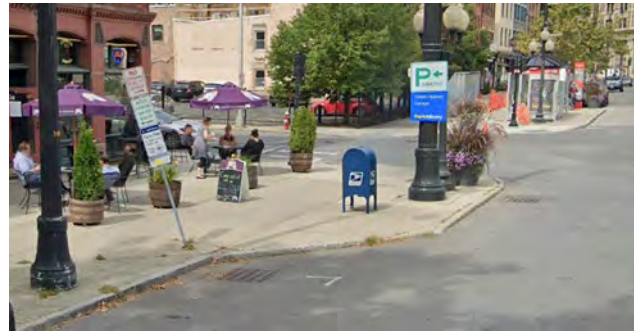
Pedestrian Refuge Island:

An area within an intersection where people wait for a break in traffic to safely cross the remainder of the intersection. Curb ramps in refuge islands must meet all the accessibility criteria of curb ramps not in refuge islands.



Curb Extension:

Curb extensions, also known as bulb-outs or bump outs, provide a clear view of oncoming traffic prior to leaving the sidewalk. Curb ramps on curb extensions must meet all accessibility criteria of curb ramps not on extensions.



APPENDIX A.4: DETECTABLE WARNING SURFACE (DWS) TYPE

Truncated Domes: Elevated, bubble-like domes on the surface of the DWS. The domes should have an approximate height of 0.2 (1/5th) inches. The domes should face the same direction as the sidewalk.

Pavement Grooves: Grooves cut into the existing concrete, facing the same direction as the sidewalk. Pavement grooves are common but are not recognized as an acceptable DWS type because they do not provide a visual contrast between the pavement and ramp.



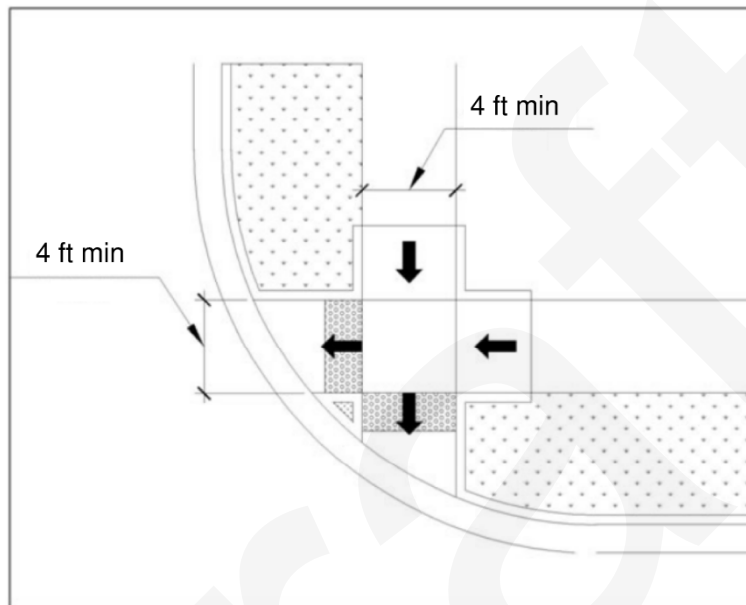
Truncated domes



Pavement grooves

APPENDIX A.5: DETECTABLE WARNING SURFACE (DWS) WIDTH, DEPTH, PLACEMENT, AND DETERIORATION

Width: The DWS should extend the full width of the curb ramp or sidewalk. The minimum width is 4 feet (48 inches).

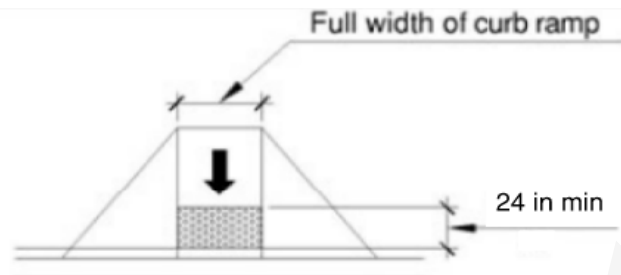


Design Guideline

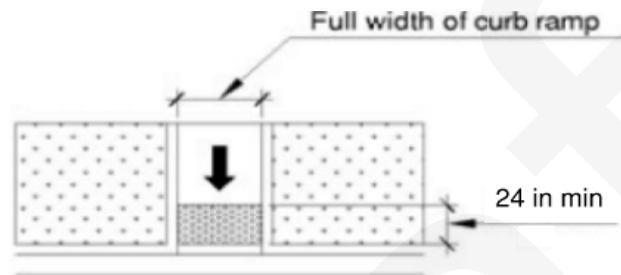


Example

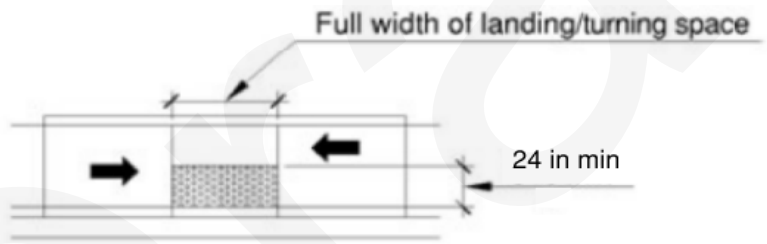
Depth: The DWS should be a minimum of 2 feet (24 inches) deep, in the direction of travel.



(a) perpendicular curb ramp with sloped edges



(b) perpendicular with rounded curb ramp

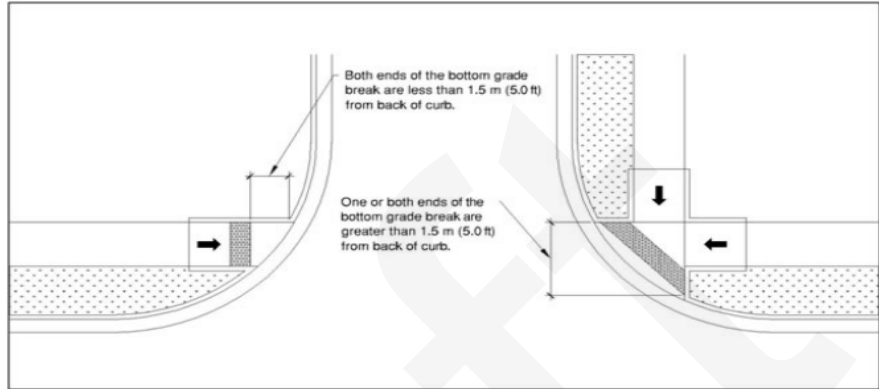


(c) parallel with curb ramp

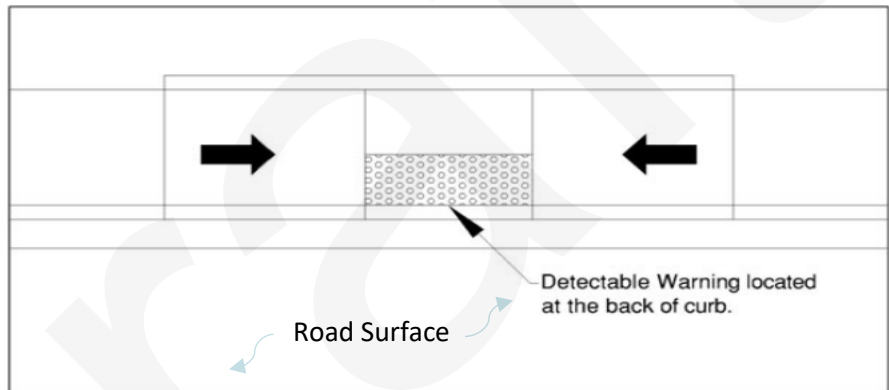
Placement: The DWS should face the same direction as the sidewalk unless the curb ramp serves two or more directions of crossing. Refer to the images below for the appropriate curb ramp.

Perpendicular curb ramp

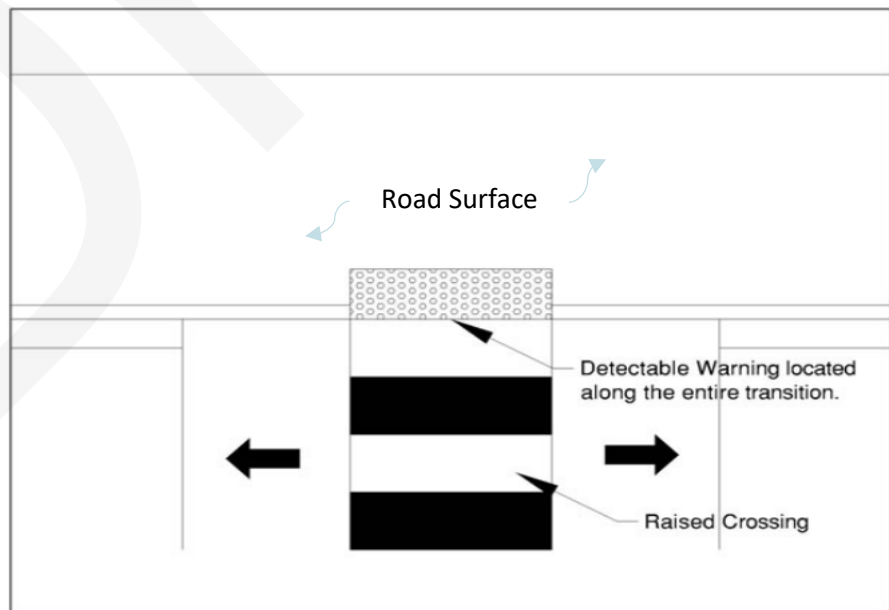
(Angled ramps are only acceptable when serving two directions at once. There must be a ramp a person can access in both directions.)



Parallel curb ramp



Blended transition



Deterioration: The DWS should be extruding from the surface so that visually impaired individuals will feel the strip.

DWS IS NOT SIGNIFICANTLY DETERIORATED



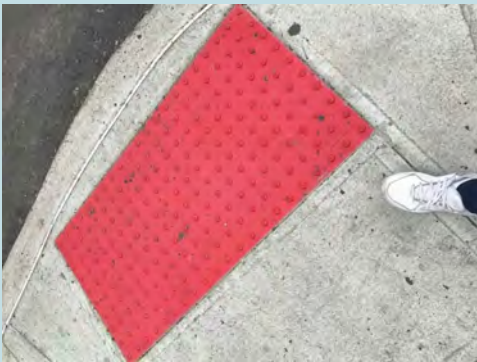
DWS IS SIGNIFICANTLY DETERIORATED



APPENDIX A.6: CONTRASTING DWS COLOR

- The color should contrast from the sidewalk, it should at least be light-on-dark or dark-on-light.
- Best practices is to install a yellow DWS (as seen on bottom left image) as it is the most visible for the vision impaired, darker colors often appear as missing gaps in the sidewalk.

DWS COLOR CONTRASTS FROM CURB RAMP



DWS COLOR DOES NOT CONTRAST FROM CURB RAMP



APPENDIX A.7: TYPE OF PEDESTRIAN SIGNAL

Pedestrian Sign – Not pedestrian activated: A sign marked for pedestrian crossing without a button to press. It may or may not have flashing lights.



Pedestrian Signal – Pedestrian activated: A button pushed by the pedestrian, which activates a crossing signal. It may or may not have a flashing light.



Features of the pedestrian crossing buttons:

Visual: Flashing lights, large/bold fonts and arrows are used.

Audible: Locating tones, or beeps, are emitted from the signal post to guide someone to the button. Alternatively, when pressed the signal will speak, communicating to the pedestrian when it is safe to cross.

Both: When a signal button uses both auditory and visual cues to aid a pedestrian in crossing the street. This includes any combination of visually high contrast signage, flashing lights/signals, or audible tones/speech.

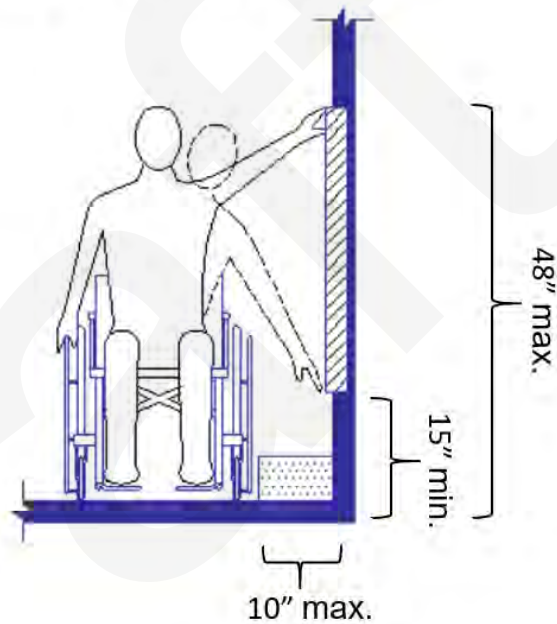
APPENDIX A.8: PEDESTRIAN SIGNAL COMPLIANCE

When the pedestrian signal includes a button, the button should be mounted within 15 inches to 48 inches (4 feet) above the ground. The button should be accessible with a maximum of a 10 inches side reach.

Accessible Pedestrian Pushbuttons

Reach Ranges

- 48" max. above the ground
- 15" min. above the ground
- Side reach within 10"



APPENDIX B.1: CROSSWALK MATERIAL

Crosswalk markings should be smooth and even with the road surface. The material of the crosswalk may differ from the typical pavement striping for aesthetic purposes. Common materials used, other than paint, are brick pavers, stamped concrete, or asphalt.

Painted



Brick Pavers



Stamped Concrete



Asphalt



APPENDIX B.2: CROSSWALK RATING

Rating	Condition	Reference Images
Poor	Crosswalk is heavily faded, completely faded, or missing sections.	
Fair	Crosswalk is fading but still visible.	
Good	Crosswalk is in like-new condition with minor cracks.	
Excellent	Crosswalk is new with no cracks or faults.	

APPENDIX C.1: SIDEWALK DEFECTS

Missing Panels



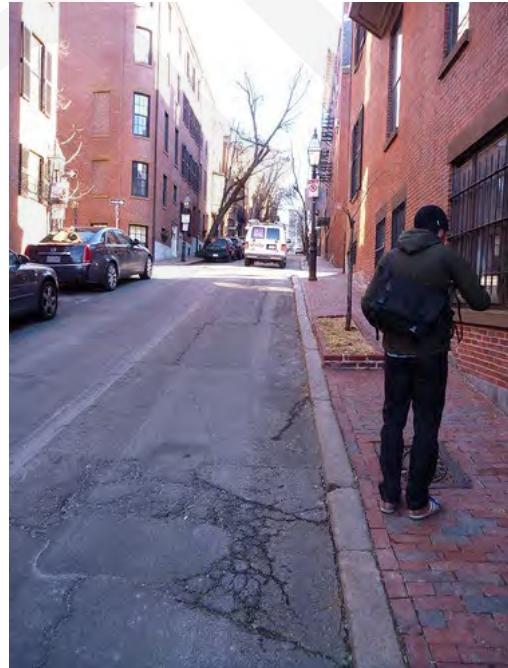
Heaving Panels




Obstructions



Too Narrow



APPENDIX C.2: SIDEWALK SEGMENT RATING

Rating	Condition	Reference Images
Fully Accessible	<ul style="list-style-type: none"> • Sidewalk is new or in like-new condition. • Sidewalk is level, no cracks, no faults, no obstructions, and adequate width. • Sidewalk is at least 4 feet (48 inches) wide. 	
Mostly Accessible	<ul style="list-style-type: none"> • Sidewalk is in almost new or good condition. • Sidewalk is level and may have minor cracks. • Sidewalk is at least 4 feet (48 inches) wide. 	

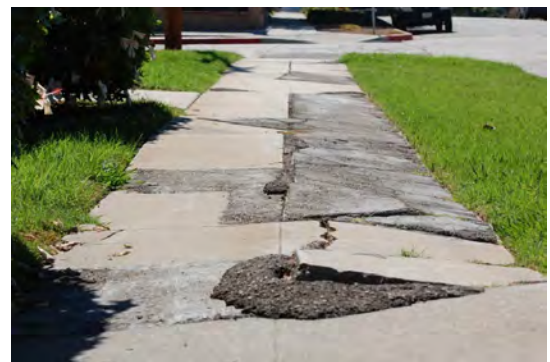
Partially
Accessible

- Small patches of broken or chipped sidewalk, and a small buildup of dirt/debris, vegetation growing between panels or cracks.
- Sidewalk may have small vertical faults larger than ½ inch but could potentially be grind down/repaired.
- Large cracks are present.
- Sidewalk is less than 4 feet wide.



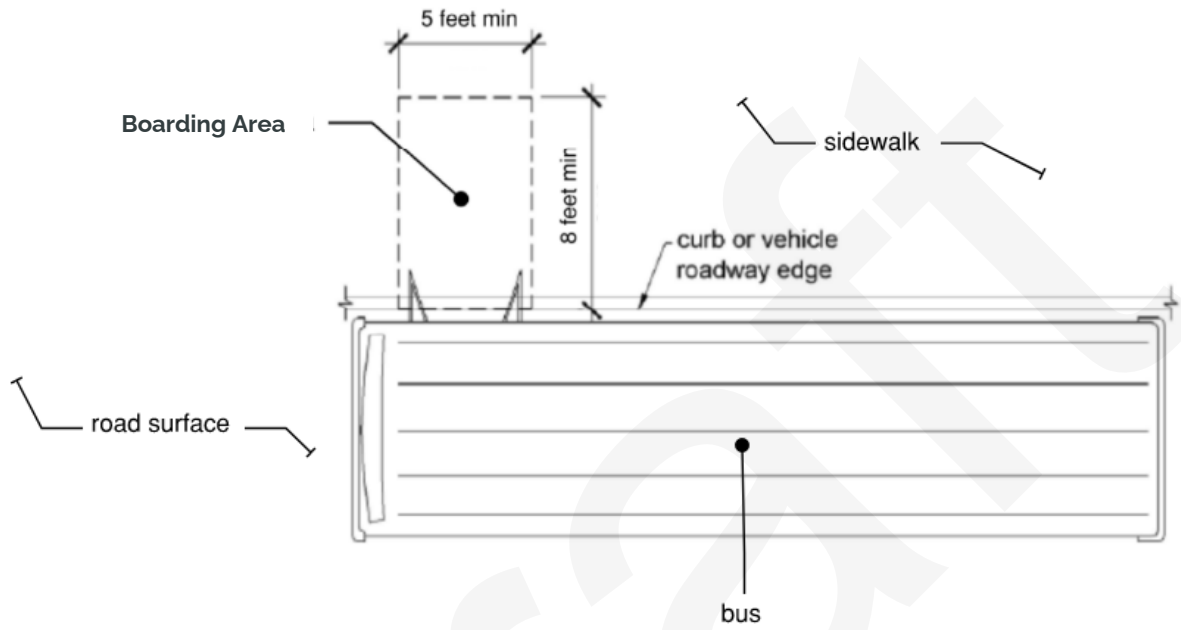
Not
Accessible

- Sidewalk may have serious accumulation of dirt/debris, establishment of vegetation or other obstructions.
- Sidewalk is uneven from faulting (greater than ½ inch) that cannot be grind down/repaired and will require replacement.
- Sidewalk is less than 4 feet wide.



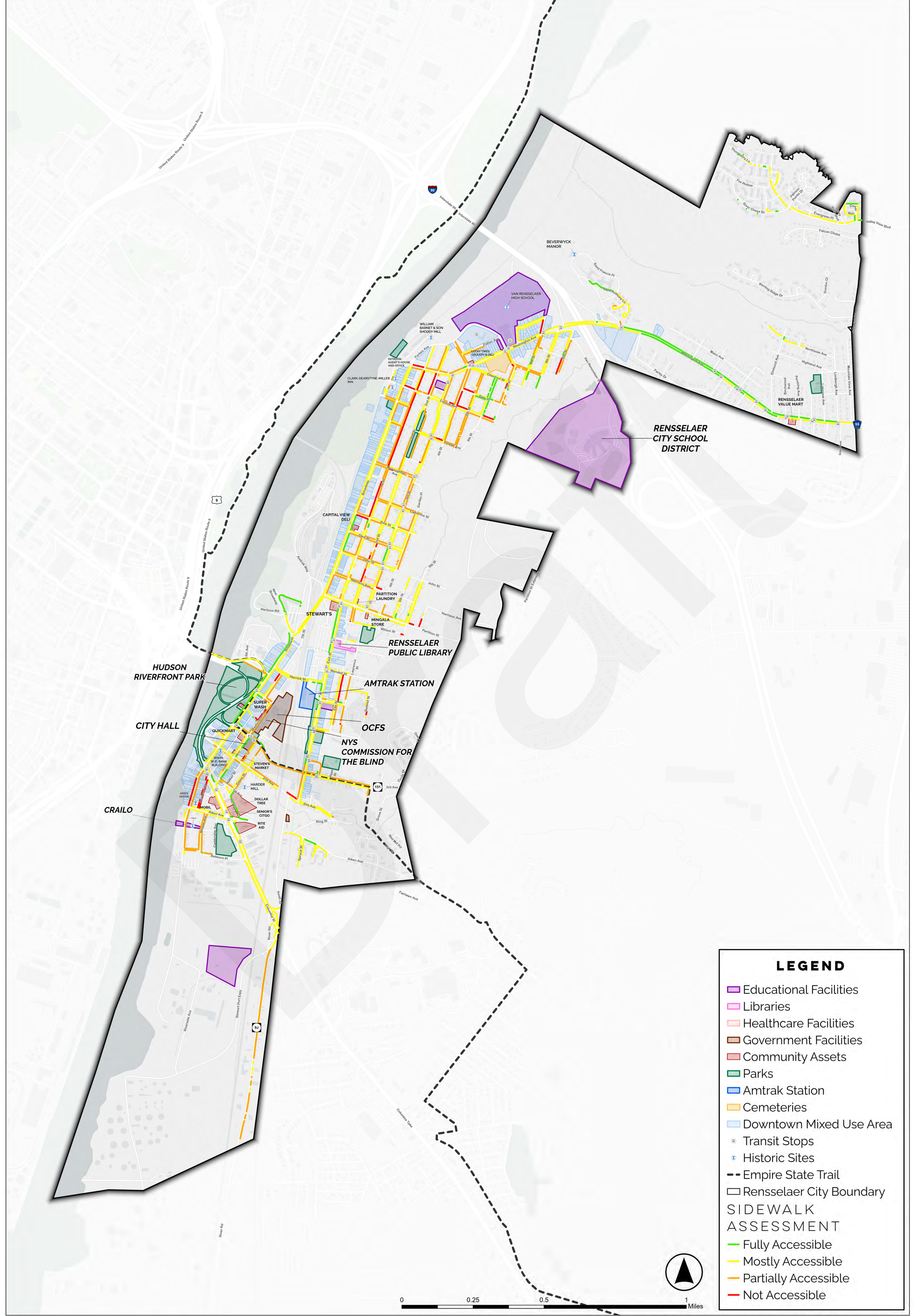
APPENDIX D: TRANSIT STOPS

Accessible transit stops require 5 feet (60 inch) wide and 8 feet (96 inch) deep boarding area.



APPENDIX B: MAPS

Draft



RENSSELAER
CITY SCHOOL
DISTRICT

HUDSON
RIVERFRONT PARK

CITY HALL

CRAILO

RENSSELAER
PUBLIC LIBRARY

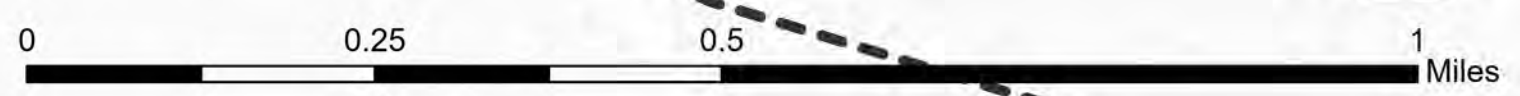
AMTRAK STATION

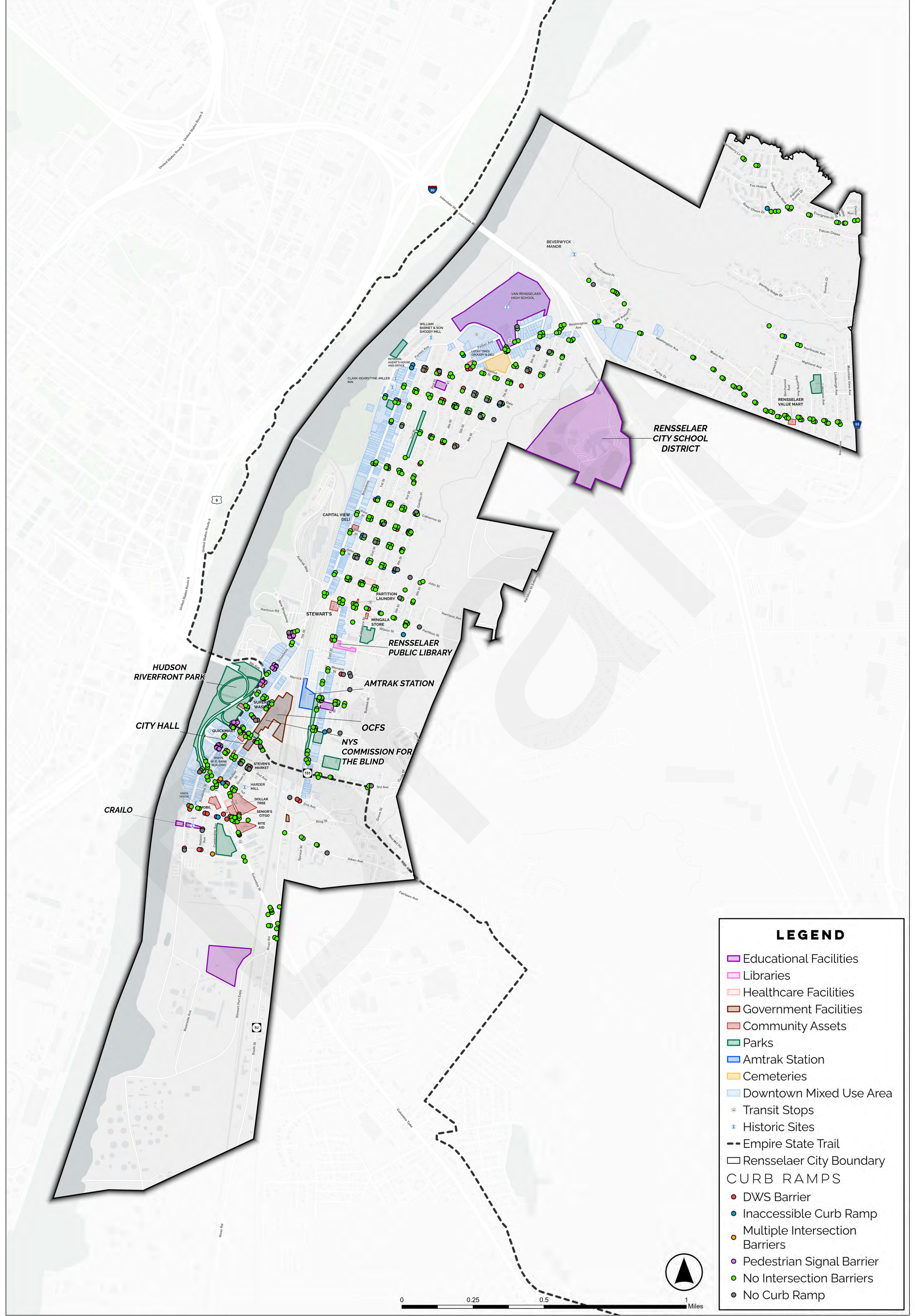
OCFS

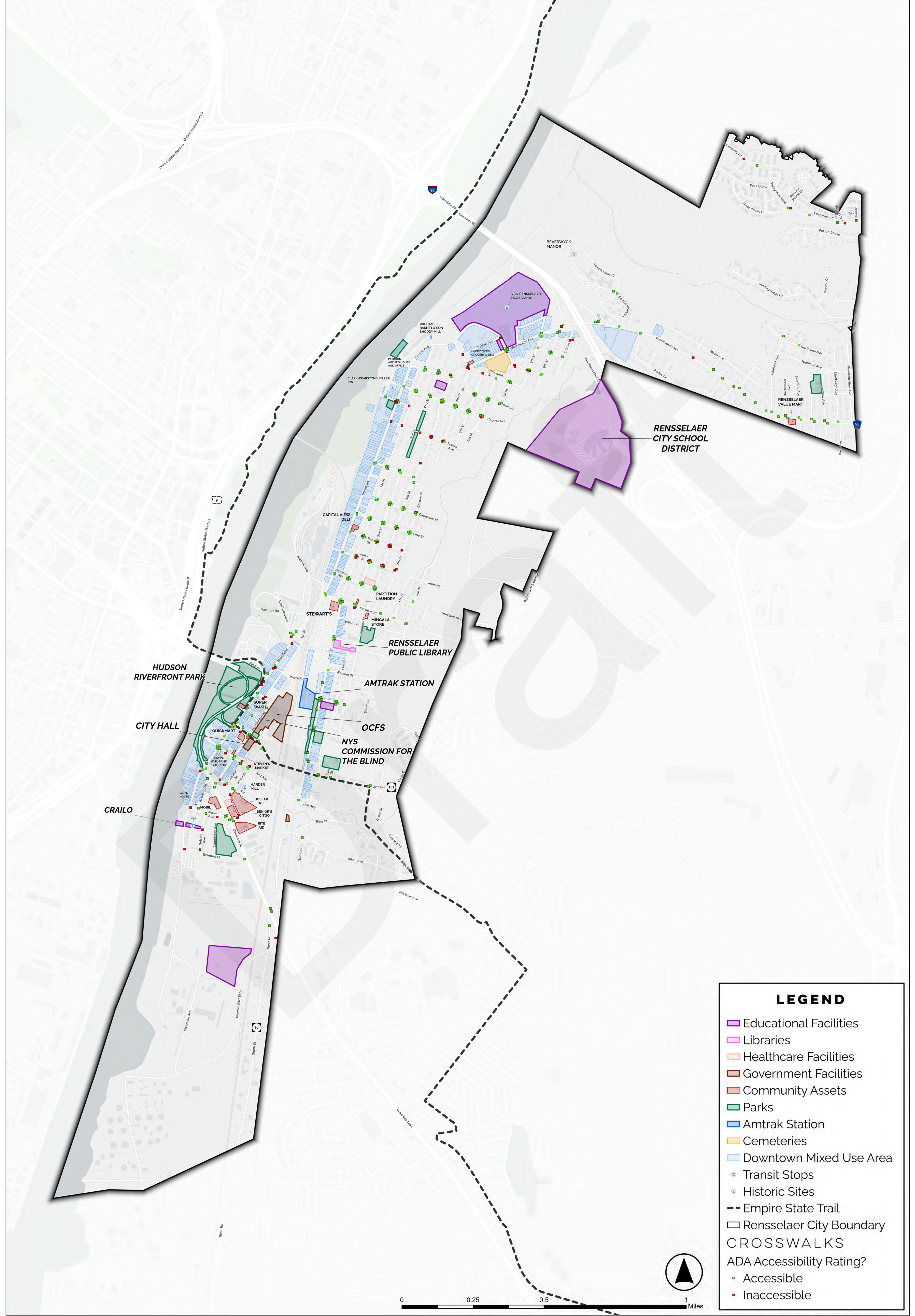
NYS
COMMISSION FOR
THE BLIND

LEGEND

- Educational Facilities
- Libraries
- Healthcare Facilities
- Government Facilities
- Community Assets
- Parks
- Amtrak Station
- Cemeteries
- Downtown Mixed Use Area
- Transit Stops
- Historic Sites
- Empire State Trail
- Rensselaer City Boundary
- SIDEWALK ASSESSMENT**
- Fully Accessible
- Mostly Accessible
- Partially Accessible
- Not Accessible





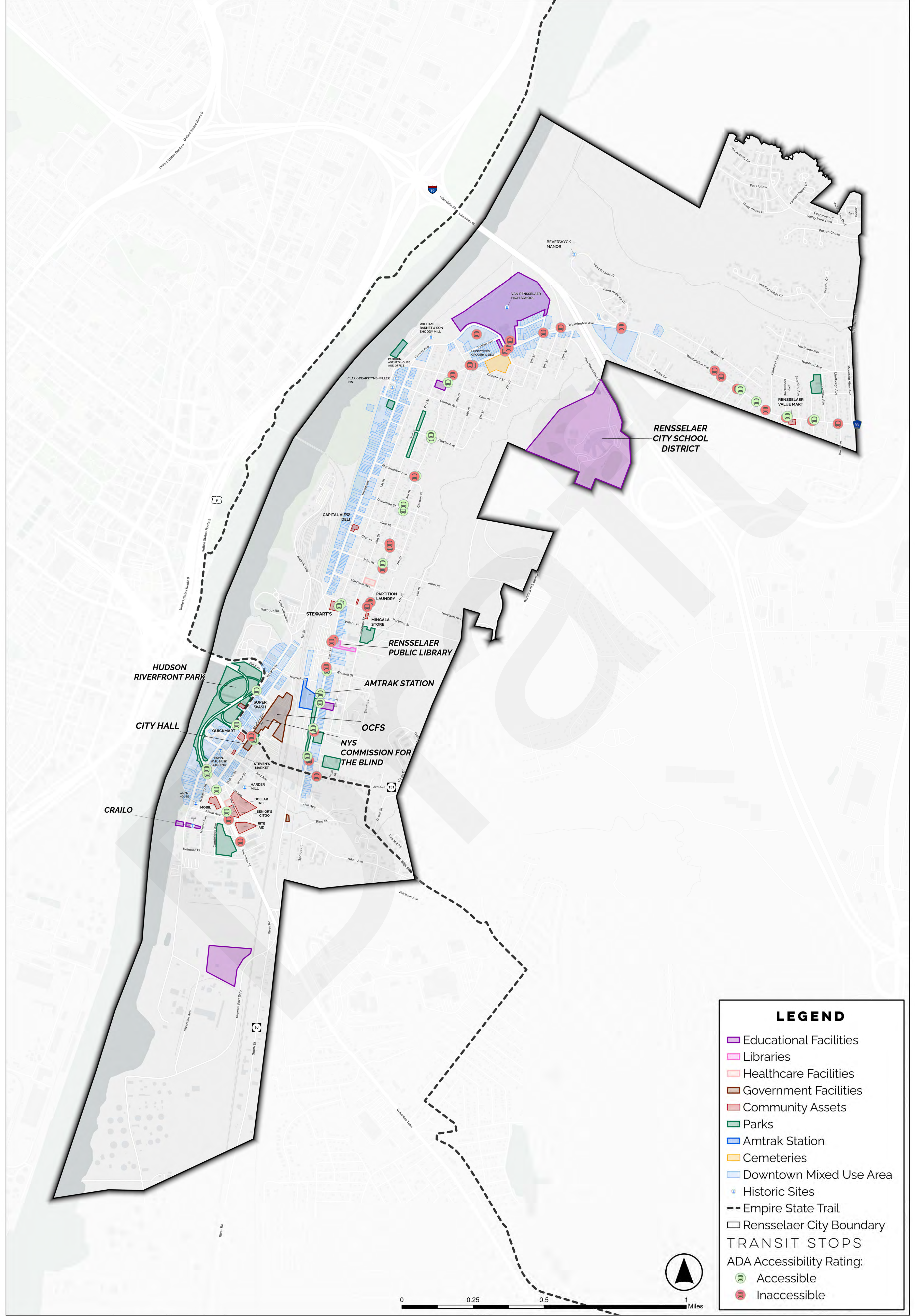


LEGEND

- Educational Facilities
- Libraries
- Healthcare Facilities
- Government Facilities
- Community Assets
- Parks
- Amtrak Station
- Cemeteries
- Downtown Mixed Use Area
- Transit Stops
- Historic Sites
- Empire State Trail
- Rensselaer City Boundary

CROSSWALKS

- ADA Accessibility Rating? Accessible
- Inaccessible



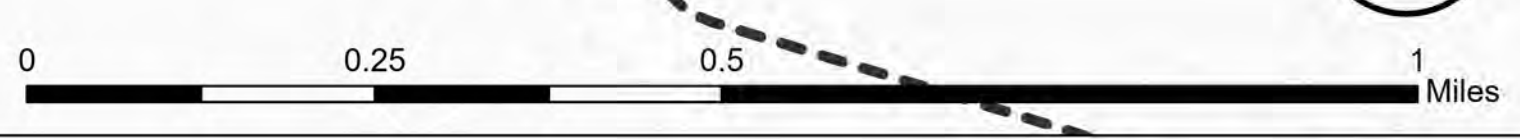
LEGEND

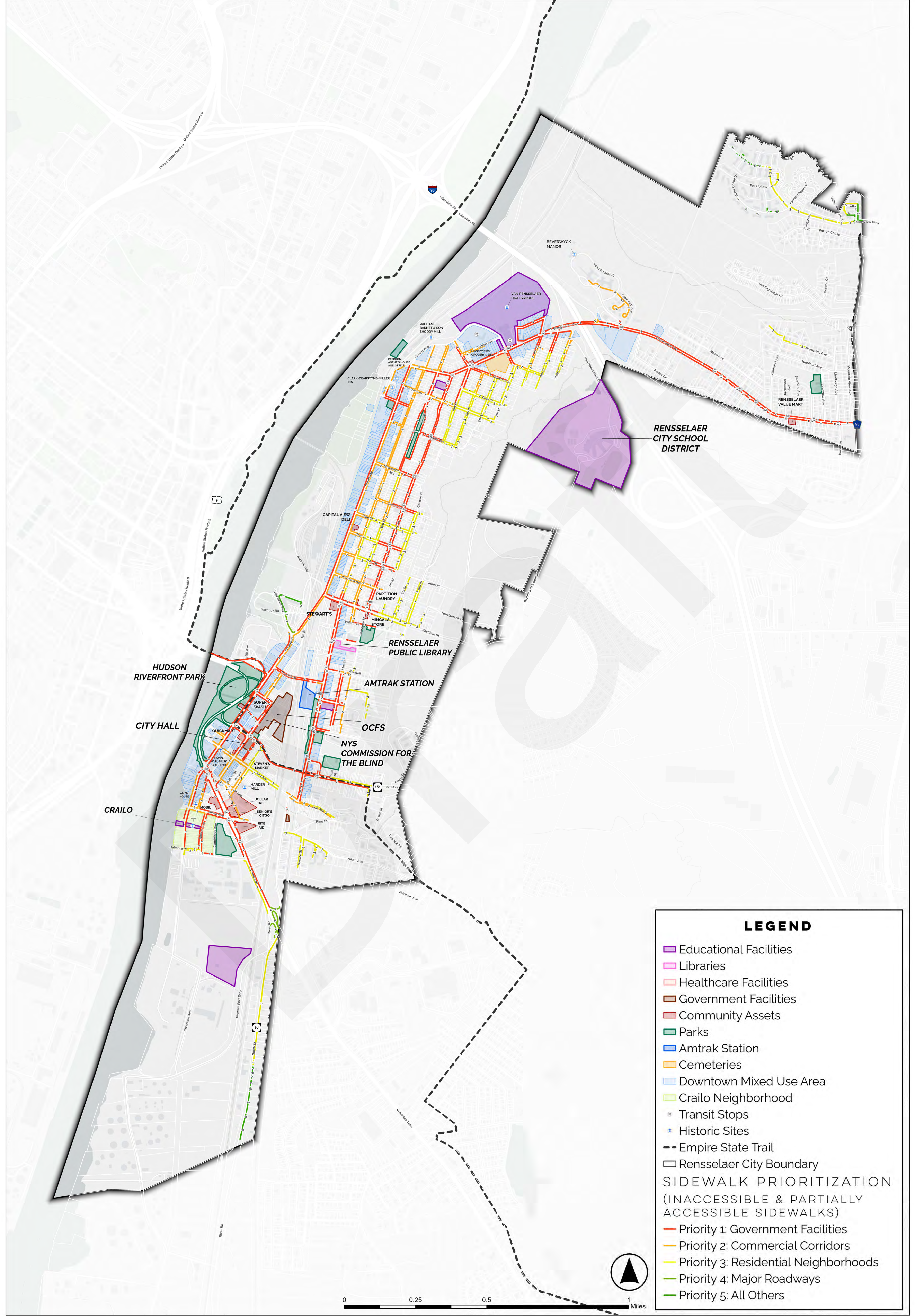
- Educational Facilities
- Libraries
- Healthcare Facilities
- Government Facilities
- Community Assets
- Parks
- Amtrak Station
- Cemeteries
- Downtown Mixed Use Area
- + Historic Sites
- Empire State Trail
- Rensselaer City Boundary

TRANSIT STOPS

ADA Accessibility Rating:

- ⊕ Accessible
- ⊕ Inaccessible



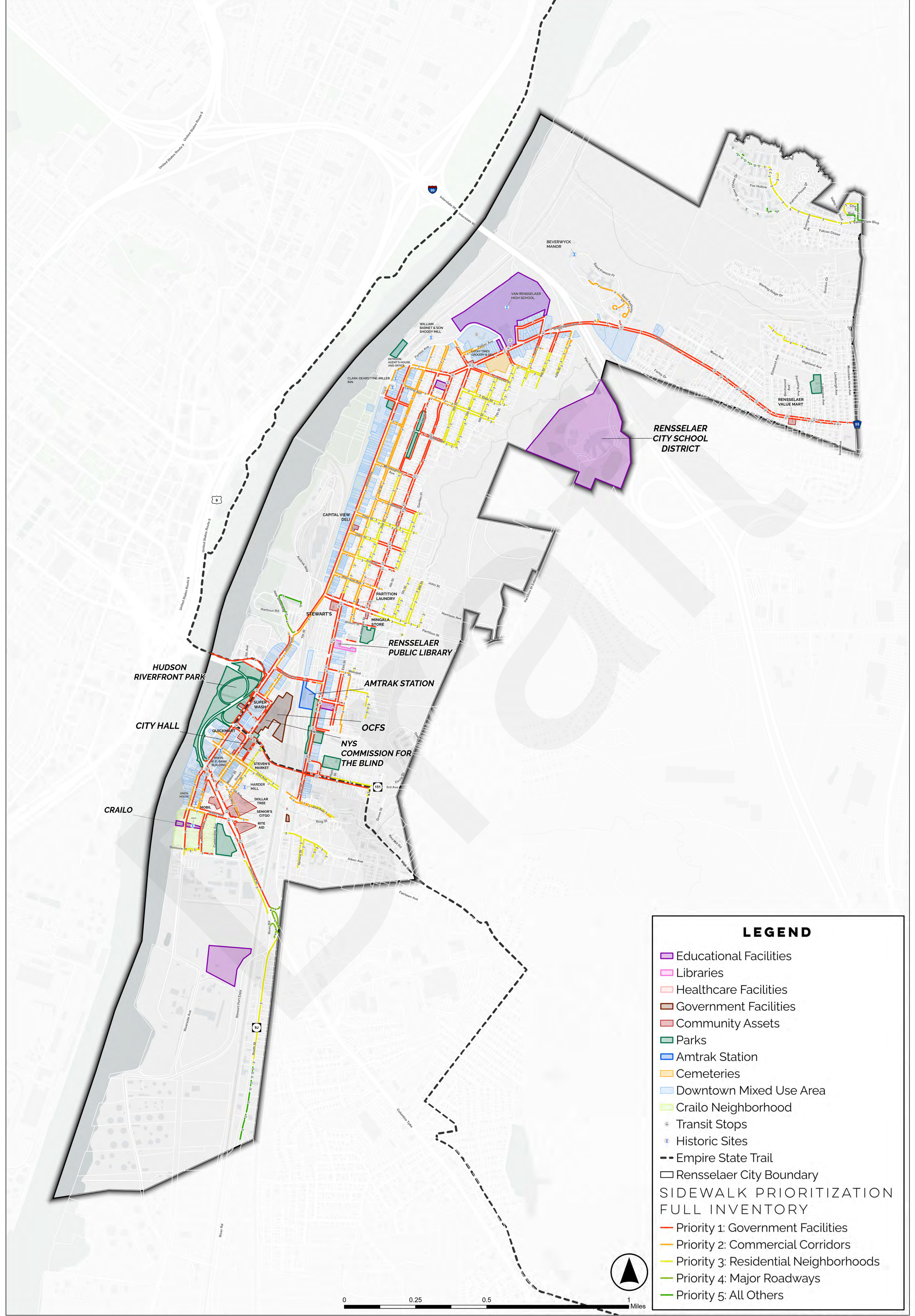


LEGEND

- Educational Facilities
- Libraries
- Healthcare Facilities
- Government Facilities
- Community Assets
- Parks
- Amtrak Station
- Cemeteries
- Downtown Mixed Use Area
- Crailo Neighborhood
- Transit Stops
- Historic Sites
- Empire State Trail
- Rensselaer City Boundary

SIDWALK PRIORITIZATION (INACCESSIBLE & PARTIALLY ACCESSIBLE SIDWALKS)

- Priority 1: Government Facilities
- Priority 2: Commercial Corridors
- Priority 3: Residential Neighborhoods
- Priority 4: Major Roadways
- Priority 5: All Others

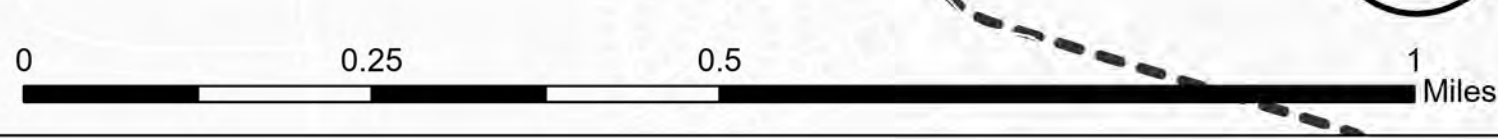


LEGEND

- Educational Facilities
- Libraries
- Healthcare Facilities
- Government Facilities
- Community Assets
- Parks
- Amtrak Station
- Cemeteries
- Downtown Mixed Use Area
- Crailo Neighborhood
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SIDEWALK PRIORITIZATION FULL INVENTORY

- Priority 1: Government Facilities
- Priority 2: Commercial Corridors
- Priority 3: Residential Neighborhoods
- Priority 4: Major Roadways
- Priority 5: All Others



APPENDIX C: COST ESTIMATES

Unit cost estimates are established using replacement costs from the NYSDOT Quick Estimator Reference – Upstate. Unit cost estimates are used in conjunction with field data to estimate repair costs. Note that specific costs may vary due to site conditions that may require relocating utilities/mailboxes; alteration of drainage structures; driveway aprons; acquiring right-of-way; pruning/clearing & grubbing; planting. Typical sidewalk replacement costs (2023) are observed at \$13 to \$15 per square foot, or \$65 to \$75 per linear foot (LF), for the purpose of budgeting in this report \$75 is recommended to assess cost. Additionally, based on jurisdiction outlined in Table 1 of this report, Responsible Entity for Maintenance of Pedestrian Facilities, costs of repairs for curb ramps, pedestrian signals, and crosswalks below only include right of ways under the jurisdiction and responsibility of the City. Note that this is a cost estimation tool, and according to City policies, sidewalk repairs will be performed at the expense of adjacent landowners unless otherwise determined by the City.

Table 1 Unit Cost Estimates

Facility	Cost
Concrete Sidewalk (4" thick, 5' wide)	\$75/linear foot
DWS	\$2,000
ADA Curb Ramp	\$6,300
LS Type Ladder Crosswalk (700ft Typical)	\$1,400
Pedestrian Signal	\$8,200

Table 2 City of Rensselaer ADA Compliance Estimated Costs

Sidewalks Estimated Cost for Compliance:	
Partially Accessible (8,649.17 ft)*	\$718,187.58
Not Accessible (3,843.59 ft)**	\$301,394.48
Total Non-ADA Compliant Segments (12,392.76 ft)	\$1,019,582.06
<i>*Assumed 15% of panels in need of replacement</i>	
<i>**Assumed 30% of panels in need of replacement</i>	
Curb Ramps and Signals Estimated Costs for Compliance:	
71 Curb Ramps*	\$447,300.00
275 DWS	\$550,000.00

17 Pedestrian Signals	\$139,400.00
Total Intersections:	\$1,136,700.00
<i>*Excludes "Not Flush with Asphalt"</i>	
Crosswalks Estimated Costs for Compliance:	
Total Crosswalks*	\$32,200
<i>*Assumed incomplete access route is addressed in curb ramp replacements</i>	
Total Capital Investment	\$ 2,188,482.06

Draft

APPENDIX D: WORK PLAN AND DATA INVENTORY

The City of Rensselaer is committed to ensuring facilities in the public right of way are accessible for everyone, including people with disabilities. The City will prioritize:

1. Facilities and intersections serving Government Facilities
2. Facilities and intersections along Commercial Corridors
3. Facilities and intersections in Residential Neighborhoods.
4. Facilities and intersections along Major Roadways
5. Facilities and intersections near all other City locations.

The facilities that are prioritized first are most likely to be frequented by residents and visitors and are essential for the City's continued operation and success. A map of all sidewalk segments by priority ranking is in Appendix B.

The "Sidewalk Segments in Priority Areas" table within this appendix will be used to identify sidewalk segments eligible for repair/replacement based on their priority ranking. Sidewalk segments will then be cross referenced with field analysis results to determine specific barriers to accessibility and the repair/replacement needs for all facilities associated with the segment (a guide to using the field analysis results is included within this appendix). The City will then work with adjacent landowners to remove barriers to accessibility until each segment is brought to compliance, beginning with the highest priority (1), and ending with the lowest priority (5). For example: sidewalk segments ranked in priority category 1 will be first to be repaired/replaced, followed by segments ranked in category 2, and so on. The same process will be used to bring intersections, and crossings, to compliance using the corresponding tables within this appendix.

The City may deviate from this framework to take advantage of regular capital improvement projects or private development to upgrade deficient facilities, if occurring adjacent to those facilities. For example: a low priority that is rated "Not Accessible" may be brought to compliance in conjunction with an adjacent road resurfacing project before the City has finished bringing higher priority segments to compliance.

As previously discussed under the Jurisdiction section of this report, if another jurisdiction is undertaking alterations of a roadway, that agency is responsible for the replacement of removed pedestrian facilities or for the construction or rehabilitation of a curb ramp to the adjacent sidewalk if one does not exist or has become inaccessible. The NYS Department of Transportation (NYSDOT) has completed a Transition Plan including sidewalks along state roadways and undertakes on-going road maintenance and alterations. For the purposes of this Transition Plan, areas lacking the provision of curb ramps in state rights-of-way will be identified in this plan as being undertaken by NYSDOT. Pedestrian signals and crosswalks on state roadways will also be identified as being undertaken by NYSDOT. The improvement of these facilities will correspond to the approved NYSDOT capital improvement program.

The table below is intended to be used as an identification tool for existing barriers and is therefore inclusive of all sidewalk segments in need of improvement, regardless of jurisdiction. For those sidewalk sections falling along County or State Roads, the City will coordinate with NYSDOT or the responsible entity to address repairs.

Draft

Sidewalks

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Not Accessible	Academy St	E	1	No	Yes	No	No
Not Accessible	Academy St	W	1	Yes	No	No	No
Not Accessible	Walker St	W	1	No	No	Yes	No
Not Accessible	Elm St	E	1	Yes	No	Yes	No
Not Accessible	Huyck Sq	N	1	No	No	Yes	No
Not Accessible	Washington St	W	1	Yes	Yes	Yes	No
Not Accessible	Fowler Ave	N	1	Yes	No	No	No
Not Accessible	Second St	W	1	Yes	Yes	Yes	No
Not Accessible	Manor Dr	W	1	Yes	No	Yes	Yes
Not Accessible	Academy St	E	2	No	Yes	Yes	No
Not Accessible	Academy St	W	2	No	Yes	Yes	No
Not Accessible	Academy St	W	2	No	Yes	Yes	Yes
Not Accessible	Broadway	W	2	Yes	No	Yes	No
Not Accessible	Academy St	W	2	Yes	No	Yes	Yes
Not Accessible	Walker St	W	2	No	No	Yes	No
Not Accessible	Second Ave	N	2	No	No	Yes	No
Not Accessible	Lansing Pl	W	2	No	No	Yes	No
Not Accessible	Washington St	W	2	No	No	Yes	No
Not Accessible	Second St	W	2	Yes	Yes	Yes	No
Not Accessible	Harrison Ave	N	2	Yes	Yes	Yes	No
Not Accessible	Pine St	S	2	Yes	No	Yes	No
Not Accessible	First St	W	2	Yes	No	Yes	No
Not Accessible	McNaughton Ave	S	2	No	No	No	No
Not Accessible	First St	E	2	Yes	Yes	Yes	Yes
Not Accessible	Central Ave	N	2	Yes	No	Yes	No
Not Accessible	First St	E	2	No	No	Yes	Yes
Not Accessible	Birch St	S	2	Yes	Yes	No	No
Not Accessible	Washington Ave	N	2	Yes	No	Yes	Yes
Not Accessible	Lawrence St	W	2	Yes	Yes	Yes	No
Not Accessible	Academy St	W	2	No	No	Yes	Yes
Not Accessible	Summit St	E	3	No	Yes	Yes	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Not Accessible	Wilson St	S	3	No	No	Yes	Yes
Not Accessible	Cottage Hill St	W	3	No	No	Yes	Yes
Not Accessible	Partition St	N	3	Yes	No	No	No
Not Accessible	Seventh St	W	3	Yes	No	Yes	Yes
Not Accessible	Second St	W	3	Yes	No	Yes	Yes
Not Accessible	Second St	E	3	Yes	Yes	No	No
Not Accessible	Church St	N	3	Yes	Yes	Yes	No
Not Accessible	Fifth St	E	3	Yes	No	Yes	No
Not Accessible	Central Ave	N	3	Yes	No	No	No
Not Accessible	Central Ave	N	3	Yes	No	No	No
Not Accessible	Central Ave	N	3	No	Yes	No	No
Not Accessible	Fourth St	E	3	Yes	Yes	No	No
Not Accessible	Dale St	N	3	Yes	No	Yes	No
Not Accessible	Seventh St	W	3	Yes	No	No	Yes
Not Accessible	Tenth St	W	3	Yes	Yes	No	No
Not Accessible			5	Yes	No	Yes	Yes
Partially Accessible	Academy St	E	1	No	No	Yes	No
Partially Accessible	Riverside Ave	E	1	No	Yes	Yes	No
Partially Accessible	Nelson Ave	W	1	No	No	Yes	Yes
Partially Accessible	Belmore Pl	N	1	No	No	No	Yes
Partially Accessible	Nelson Ave	E	1	No	No	Yes	Yes
Partially Accessible	Nelson Ave	E	1	Yes	No	No	Yes
Partially Accessible	Broadway	E	1	Yes	No	No	No
Partially Accessible	Broadway	W	1	No	No	Yes	No
Partially Accessible	Washington St	E	1	Yes	No	No	No
Partially Accessible	Walker St	W	1	Yes	No	Yes	No
Partially Accessible	Walker St	W	1	No	No	No	No
Partially Accessible	Walker St	E	1	No	No	Yes	No
Partially Accessible	Walker St	E	1	Yes	No	Yes	No
Partially Accessible	Third Ave	S	1	No	No	No	Yes
Partially Accessible	East St	W	1	Yes	No	No	No
Partially Accessible	Adams St	E	1	No	No	Yes	No
Partially Accessible	Fourth Ave	S	1	Yes	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Partially Accessible	Elm St	E	1	Yes	No	No	No
Partially Accessible	Herrick St	N	1	Yes	No	No	No
Partially Accessible	Third Ave	N	1	Yes	No	No	No
Partially Accessible	Third Ave	N	1	No	No	Yes	No
Partially Accessible	Washington St	W	1	No	No	Yes	No
Partially Accessible	Washington St	E	1	Yes	No	No	No
Partially Accessible	Fourth Ave	S	1	No	Yes	No	No
Partially Accessible	Fourth Ave	N	1	No	No	Yes	No
Partially Accessible	5th Ave	S	1	Yes	No	No	No
Partially Accessible	Partition St	N	1	Yes	No	No	Yes
Partially Accessible	First St	W	1	No	Yes	No	No
Partially Accessible	Third St	E	1	No	No	No	Yes
Partially Accessible	John St	N	1	Yes	No	No	No
Partially Accessible	Glen St	S	1	No	No	No	No
Partially Accessible	Glen St	N	1	No	No	No	No
Partially Accessible	Catherine St	N	1	Yes	Yes	No	No
Partially Accessible	Catherine St	N	1	Yes	No	No	No
Partially Accessible	Third St	E	1	No	Yes	No	No
Partially Accessible	Third St	W	1	Yes	Yes	No	No
Partially Accessible	McNaughton Ave	S	1	Yes	No	No	No
Partially Accessible	McNaughton Ave	S	1	Yes	No	No	No
Partially Accessible	Second St	W	1	Yes	No	No	No
Partially Accessible	McNaughton Ave	N	1	Yes	No	Yes	No
Partially Accessible	Third St	W	1	No	No	Yes	No
Partially Accessible	Second St	E	1	No	No	Yes	No
Partially Accessible	Second St	E	1	No	No	No	No
Partially Accessible	Second St	E	1	No	No	Yes	No
Partially Accessible	Tracy St	S	1	Yes	No	Yes	No
Partially Accessible	Broadway	E	1	Yes	No	Yes	No
Partially Accessible	Tracy St	S	1	Yes	No	Yes	No
Partially Accessible	Tracy St	N	1	Yes	No	No	No
Partially Accessible	Broadway	E	1	No	No	No	Yes
Partially Accessible	Second St	E	1	No	No	Yes	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Partially Accessible	Second St	E	1	Yes	Yes	No	No
Partially Accessible	Third St	E	1	Yes	No	No	No
Partially Accessible	Fourth St	W	1	No	No	No	Yes
Partially Accessible	Fourth St	E	1	Yes	No	No	No
Partially Accessible	Birch St	S	1	Yes	No	No	Yes
Partially Accessible	Eighth St	W	1	No	No	No	Yes
Partially Accessible	Eighth St	E	1	Yes	Yes	No	No
Partially Accessible	Washington Ave	N	1	Yes	No	No	No
Partially Accessible	Washington Ave	N	1	Yes	No	No	No
Partially Accessible	Fourth St	W	1	No	No	No	No
Partially Accessible	Fourth St	E	1	Yes	Yes	No	No
Partially Accessible	Fowler Ave	N	1	Yes	No	No	No
Partially Accessible	Fowler Ave	N	1	Yes	No	No	No
Partially Accessible	St Francis Pl	E	1	No	No	No	Yes
Partially Accessible	Third Ave	S	1	No	No	No	No
Partially Accessible	Adams St	N	1	No	No	Yes	No
Partially Accessible	Third Ave	N	1	Yes	No	No	No
Partially Accessible	First Ave	S	2	No	No	Yes	Yes
Partially Accessible	First Ave	N	2	No	No	Yes	Yes
Partially Accessible	First Ave	N	2	Yes	No	No	Yes
Partially Accessible	Green St	E	2	Yes	Yes	No	No
Partially Accessible	Second Ave	S	2	No	No	Yes	No
Partially Accessible	Washington St	W	2	No	No	No	No
Partially Accessible	Second Ave	S	2	No	No	Yes	No
Partially Accessible	Ferry St	N	2	No	No	Yes	No
Partially Accessible	Ferry St	S	2	No	Yes	No	No
Partially Accessible	Academy St	E	2	No	Yes	No	No
Partially Accessible	Lawrence St	W	2	No	No	Yes	No
Partially Accessible	Lawrence St	W	2	Yes	No	Yes	No
Partially Accessible	Lawrence St	E	2	Yes	No	No	No
Partially Accessible	Lawrence St	W	2	No	No	Yes	No
Partially Accessible	Lansing Pl	W	2	Yes	No	No	No
Partially Accessible	Lansing Pl	E	2	Yes	No	Yes	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Partially Accessible	Washington St	E	2	No	No	No	Yes
Partially Accessible	Harrison Ave	S	2	Yes	Yes	Yes	No
Partially Accessible	Harrison Ave	S	2	Yes	No	Yes	No
Partially Accessible	Broadway	E	2	No	No	Yes	No
Partially Accessible	Harrison Ave	N	2	Yes	No	Yes	No
Partially Accessible	Harrison Ave	N	2	No	No	No	Yes
Partially Accessible	Second St	E	2	No	No	No	Yes
Partially Accessible	John St	N	2	Yes	No	No	No
Partially Accessible	First St	W	2	Yes	No	No	No
Partially Accessible	Glen St	S	2	Yes	No	No	No
Partially Accessible	First St	W	2	No	Yes	No	No
Partially Accessible	Broadway	E	2	Yes	No	No	No
Partially Accessible	Pine St	N	2	Yes	No	No	No
Partially Accessible	First St	E	2	Yes	No	No	No
Partially Accessible	Catherine St	N	2	No	No	Yes	No
Partially Accessible	Catherine St	S	2	Yes	No	Yes	No
Partially Accessible	Second St	E	2	No	No	Yes	Yes
Partially Accessible	First St	W	2	No	No	No	Yes
Partially Accessible	McNaughton Ave	S	2	No	No	Yes	No
Partially Accessible	McNaughton Ave	N	2	Yes	No	No	No
Partially Accessible	First St	W	2	Yes	No	Yes	Yes
Partially Accessible	Fowler Ave	S	2	Yes	No	Yes	No
Partially Accessible	First St	W	2	Yes	No	Yes	Yes
Partially Accessible	Forbes Ave	E	2	No	No	No	No
Partially Accessible	First St	W	2	Yes	Yes	No	No
Partially Accessible	Washington Ave	S	2	Yes	No	No	No
Partially Accessible	Central Ave	N	2	Yes	No	No	No
Partially Accessible	Central Ave	N	2	Yes	No	No	No
Partially Accessible	Central Ave	N	2	Yes	Yes	No	No
Partially Accessible	Central Ave	S	2	Yes	No	Yes	No
Partially Accessible	Fifth St	W	2	Yes	Yes	No	No
Partially Accessible	Fifth St	E	2	Yes	No	No	Yes
Partially Accessible	Birch St	N	2	No	No	Yes	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Partially Accessible	Ash St	N	2	No	No	No	Yes
Partially Accessible	Tenth St	E	2	Yes	No	No	No
Partially Accessible	Washington Ave	N	2	Yes	No	Yes	No
Partially Accessible	Second St	W	2	Yes	No	Yes	No
Partially Accessible	Fourth St	E	2	Yes	No	Yes	No
Partially Accessible	Patten Ave	S	2	Yes	No	Yes	Yes
Partially Accessible	Forbes Ave	E	2	No	Yes	No	No
Partially Accessible	Bishop Hubbard Cir	N	2	No	No	No	No
Partially Accessible	South St	E	3	Yes	No	Yes	No
Partially Accessible	Belmore Pl	S	3	No	No	Yes	No
Partially Accessible	Belmore Pl	S	3	No	No	Yes	No
Partially Accessible	Belmore Pl	N	3	No	No	No	Yes
Partially Accessible	Green St	E	3	Yes	No	No	No
Partially Accessible	Second Ave	N	3	No	Yes	No	No
Partially Accessible	Green St	W	3	No	Yes	Yes	No
Partially Accessible	High St	W	3	Yes	No	No	No
Partially Accessible	Second Ave	N	3	Yes	No	No	No
Partially Accessible	Partition St	N	3	Yes	No	No	Yes
Partially Accessible	Fourth St	E	3	Yes	No	Yes	No
Partially Accessible	First St	E	3	No	No	No	No
Partially Accessible	Second St	W	3	No	No	No	Yes
Partially Accessible	John St	S	3	No	No	Yes	Yes
Partially Accessible	John St	N	3	No	No	No	Yes
Partially Accessible	Second St	W	3	Yes	No	No	No
Partially Accessible	Second St	W	3	Yes	No	No	No
Partially Accessible	Second St	W	3	No	No	No	No
Partially Accessible	Second St	E	3	Yes	No	Yes	Yes
Partially Accessible	Fourth St	W	3	Yes	No	No	No
Partially Accessible	Fourth St	W	3	No	No	Yes	Yes
Partially Accessible	Fourth St	E	3	Yes	No	No	No
Partially Accessible	Glen St	S	3	Yes	No	No	No
Partially Accessible	Glen St	N	3	No	No	Yes	No
Partially Accessible	Pine St	S	3	Yes	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Partially Accessible	McNaughton Ave	S	3	No	No	Yes	No
Partially Accessible	Pine St	N	3	Yes	No	No	No
Partially Accessible	Fourth St	W	3	No	No	Yes	No
Partially Accessible	Catherine St	N	3	Yes	No	No	No
Partially Accessible	Fowler Ave	S	3	No	No	No	Yes
Partially Accessible	Fowler Ave	S	3	No	No	Yes	No
Partially Accessible	Fowler Ave	N	3	No	No	No	Yes
Partially Accessible	Central Ave	S	3	Yes	No	No	No
Partially Accessible	Central Ave	S	3	Yes	No	Yes	No
Partially Accessible	Fifth St	E	3	Yes	No	Yes	Yes
Partially Accessible	Fifth St	E	3	No	No	Yes	No
Partially Accessible	Fifth St	W	3	Yes	No	No	No
Partially Accessible	Fifth St	W	3	Yes	No	Yes	Yes
Partially Accessible	Fowler Ave	N	3	Yes	No	Yes	No
Partially Accessible	Fourth St	E	3	Yes	No	Yes	No
Partially Accessible	Fifth St	W	3	No	No	Yes	No
Partially Accessible	Fifth St	W	3	Yes	Yes	No	No
Partially Accessible	Dale St	N	3	Yes	No	No	No
Partially Accessible	Seventh St	W	3	No	No	No	No
Partially Accessible	Seventh St	W	3	Yes	Yes	No	No
Partially Accessible	Seventh St	W	3	No	No	No	No
Partially Accessible	Seventh St	W	3	Yes	No	No	No
Partially Accessible	Seventh St	E	3	Yes	No	No	No
Partially Accessible	Seventh St	E	3	Yes	No	No	No
Partially Accessible	Dale St	N	3	Yes	No	No	No
Partially Accessible	Birch St	S	3	Yes	No	No	No
Partially Accessible	Birch St	S	3	Yes	No	No	Yes
Partially Accessible	Birch St	S	3	Yes	No	No	No
Partially Accessible	Seventh St	E	3	Yes	No	No	No
Partially Accessible	Seventh St	W	3	Yes	No	Yes	Yes
Partially Accessible	Birch St	N	3	Yes	Yes	No	No
Partially Accessible	Tenth St	E	3	Yes	No	Yes	No
Partially Accessible	First St	W	3	No	No	No	Yes

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Partially Accessible	Fifth St	E	3	Yes	No	Yes	No
Partially Accessible	Third Ave	N	3	Yes	No	No	No
Partially Accessible	South St	E	5	No	No	Yes	Yes
Partially Accessible	South St	E	5	Yes	No	Yes	No
Partially Accessible	South St	E	5	No	No	Yes	No
Partially Accessible	South St	E	5	Yes	No	No	No
Partially Accessible	South St	E	5	Yes	No	Yes	No
Partially Accessible			5				
Partially Accessible			5	Yes	No	Yes	No
Mostly Accessible	Dunn Memorial Bridge	N	1	No	No	No	No
Mostly Accessible	Dunn Memorial Bridge	E	1	No	No	No	No
Mostly Accessible	Dunn Memorial Bridge	N	1	No	No	No	No
Mostly Accessible	Columbia St	E	1	No	No	No	No
Mostly Accessible	Aiken Ave	S	1	No	No	No	No
Mostly Accessible	Aiken Ave	S	1	No	No	No	No
Mostly Accessible	Columbia St	E	1	No	No	No	No
Mostly Accessible	Washington St	E	1	No	No	No	No
Mostly Accessible	Columbia St	E	1	No	No	No	No
Mostly Accessible	Columbia St	W	1	No	No	No	No
Mostly Accessible	Columbia St	W	1	No	No	No	No
Mostly Accessible	Columbia St	W	1	No	No	No	No
Mostly Accessible	Columbia St	W	1	No	No	No	No
Mostly Accessible	Riverside Ave	W	1	No	No	No	No
Mostly Accessible	Riverside Ave	W	1	No	No	No	No
Mostly Accessible	Riverside Ave	W	1	No	No	No	No
Mostly Accessible	Aiken Ave	S	1	No	No	Yes	No
Mostly Accessible	Aiken Ave	S	1	No	No	No	No
Mostly Accessible	Aiken Ave	N	1	No	No	No	No
Mostly Accessible	Broadway	W	1	No	No	No	No
Mostly Accessible	Aiken Ave	N	1	No	No	No	No
Mostly Accessible	Broadway	E	1	No	No	No	No
Mostly Accessible	Broadway	W	1	No	No	No	No
Mostly Accessible	Broadway	E	1	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible	Third Ave	S	1	No	No	No	No
Mostly Accessible	Third Ave	S	1	No	No	No	No
Mostly Accessible	Broadway	W	1	No	No	No	No
Mostly Accessible	Third Ave	S	1	No	No	No	No
Mostly Accessible	Third Ave	S	1	No	No	No	No
Mostly Accessible	Washington St	W	1	No	No	No	No
Mostly Accessible	Washington St	E	1	No	No	No	No
Mostly Accessible	Walker St	E	1	No	No	No	No
Mostly Accessible	Walker St	E	1	No	No	No	No
Mostly Accessible	East St	E	1	No	No	No	No
Mostly Accessible	East St	E	1	No	No	No	No
Mostly Accessible	East St	W	1	No	No	No	No
Mostly Accessible	Fourth Ave	N	1	No	No	Yes	No
Mostly Accessible	Elm St	W	1	No	No	No	No
Mostly Accessible	Herrick St	S	1	No	No	No	No
Mostly Accessible	Herrick St	S	1	No	No	No	No
Mostly Accessible	Herrick St	N	1	No	No	No	No
Mostly Accessible	Elm St	W	1	No	No	No	No
Mostly Accessible	East St	E	1	No	No	No	No
Mostly Accessible	East St	W	1	No	No	No	No
Mostly Accessible	Wendell St	N	1	No	No	No	No
Mostly Accessible	Willow St	N	1	No	No	Yes	No
Mostly Accessible	East St	E	1	No	No	No	No
Mostly Accessible	East St	E	1	No	No	No	No
Mostly Accessible	Partition St	S	1	No	No	No	No
Mostly Accessible	Wilson St	N	1	No	No	No	No
Mostly Accessible	Partition St	S	1	No	No	No	No
Mostly Accessible	Partition St	S	1	No	No	Yes	Yes
Mostly Accessible	Partition St	S	1	No	No	No	No
Mostly Accessible	Broadway	E	1	No	No	No	No
Mostly Accessible	Huyck Sq	S	1	No	No	No	No
Mostly Accessible	Broadway	E	1	No	No	No	No
Mostly Accessible	Washington St	W	1	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible	Fourth Ave	N	1	No	No	No	No
Mostly Accessible	Broadway	E	1	No	No	No	No
Mostly Accessible	Herrick St	S	1	No	No	No	No
Mostly Accessible	Herrick St	N	1	No	No	No	No
Mostly Accessible	Partition St	N	1	No	No	No	No
Mostly Accessible	Partition St	N	1	No	No	No	No
Mostly Accessible	Third St	E	1	No	No	No	No
Mostly Accessible	Third St	W	1	No	No	No	No
Mostly Accessible	Partition St	N	1	No	No	No	No
Mostly Accessible	First St	E	1	No	No	No	No
Mostly Accessible	Third St	W	1	No	No	No	No
Mostly Accessible	John St	S	1	No	No	No	No
Mostly Accessible	John St	S	1	No	No	No	No
Mostly Accessible	Third St	E	1	No	No	No	No
Mostly Accessible	John St	N	1	No	No	No	No
Mostly Accessible	Glen St	S	1	No	No	No	No
Mostly Accessible	Broadway	W	1	No	No	No	No
Mostly Accessible	Third St	W	1	No	No	No	No
Mostly Accessible	Third St	E	1	No	No	No	No
Mostly Accessible	Glen St	N	1	No	No	No	No
Mostly Accessible	Catherine St	S	1	No	No	No	No
Mostly Accessible	Third St	W	1	No	No	No	No
Mostly Accessible	Third St	E	1	No	No	No	No
Mostly Accessible	Catherine St	S	1	No	No	No	No
Mostly Accessible	Second St	W	1	No	No	No	No
Mostly Accessible	Second St	W	1	No	No	No	No
Mostly Accessible	Second St	E	1	No	No	Yes	No
Mostly Accessible	Third St	W	1	No	No	No	No
Mostly Accessible	Third St	E	1	No	No	No	No
Mostly Accessible	Fowler Ave	S	1	No	No	No	Yes
Mostly Accessible	Fowler Ave	S	1	No	No	No	No
Mostly Accessible	Third St	E	1	No	No	No	No
Mostly Accessible	Third St	W	1	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible	Washington Ave	S	1	No	No	No	No
Mostly Accessible	Washington Ave	S	1	No	No	No	No
Mostly Accessible	Washington Ave	S	1	No	No	No	No
Mostly Accessible	Washington Ave	N	1	No	No	No	No
Mostly Accessible	Third Ave	S	1	No	No	No	No
Mostly Accessible	Willow St	N	1	No	No	No	No
Mostly Accessible	Third Ave	S	1	No	No	No	No
Mostly Accessible	Third Ave	N	1	No	No	No	No
Mostly Accessible	East St	E	1	No	No	No	No
Mostly Accessible	Partition St	S	1	No	No	No	No
Mostly Accessible	Washington St	N	2	No	No	Yes	No
Mostly Accessible	Aiken Ave	S	2	No	No	No	No
Mostly Accessible	Aiken Ave	N	2	No	No	No	No
Mostly Accessible	Broadway	W	2	No	No	No	No
Mostly Accessible	1st Alley	W	2	No	No	No	No
Mostly Accessible	Walker St	E	2	No	No	No	No
Mostly Accessible	Green St	W	2	No	No	No	No
Mostly Accessible	Second Ave	S	2	No	No	No	No
Mostly Accessible	Second Ave	S	2	No	No	No	No
Mostly Accessible	Second Ave	S	2	No	No	No	No
Mostly Accessible	Second Ave	N	2	No	No	No	No
Mostly Accessible	Broadway	W	2	No	No	No	No
Mostly Accessible	Second Ave	N	2	No	No	No	No
Mostly Accessible	Ferry St	S	2	No	No	No	No
Mostly Accessible	Second Ave	S	2	No	No	No	No
Mostly Accessible	Second Ave	N	2	No	No	Yes	No
Mostly Accessible	Second Ave	N	2	No	No	No	No
Mostly Accessible	Second Ave	N	2	No	No	No	No
Mostly Accessible	Second Ave	N	2	No	No	No	No
Mostly Accessible	Second Ave	N	2	No	No	No	No
Mostly Accessible	Second Ave	N	2	No	No	No	No
Mostly Accessible	Second Ave	N	2	No	No	No	No
Mostly Accessible	Second Ave	S	2	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible	Second Ave	S	2	No	No	Yes	No
Mostly Accessible	Herrick St	N	2	No	No	No	No
Mostly Accessible	Wendell St	N	2	No	No	No	No
Mostly Accessible	Wendell St	N	2	No	No	No	No
Mostly Accessible	Wendell St	N	2	No	No	No	No
Mostly Accessible	Partition St	S	2	No	No	No	No
Mostly Accessible	Broadway	E	2	No	No	No	No
Mostly Accessible	Washington St	W	2	No	No	No	No
Mostly Accessible	Broadway	E	2	No	No	Yes	No
Mostly Accessible	Second St	E	2	No	No	No	No
Mostly Accessible	Second St	W	2	No	No	No	No
Mostly Accessible	Harrison Ave	S	2	No	No	No	No
Mostly Accessible	First St	W	2	No	No	No	No
Mostly Accessible	First St	W	2	No	No	No	No
Mostly Accessible	Broadway	E	2	No	No	No	No
Mostly Accessible	First St	W	2	No	No	No	No
Mostly Accessible	Second St	E	2	No	No	No	No
Mostly Accessible	Harrison Ave	N	2	No	No	No	No
Mostly Accessible	Harrison Ave	N	2	No	No	No	No
Mostly Accessible		S	2	No	No	No	No
Mostly Accessible	Glen St	N	2	No	No	No	No
Mostly Accessible	First St	W	2	No	No	No	No
Mostly Accessible	Broadway	E	2	No	No	No	No
Mostly Accessible	Broadway	E	2	No	No	No	No
Mostly Accessible	Second St	W	2	No	No	No	No
Mostly Accessible	Second St	E	2	No	No	No	No
Mostly Accessible	First St	E	2	No	No	No	No
Mostly Accessible	Second St	W	2	No	No	No	No
Mostly Accessible	Broadway	E	2	No	No	No	No
Mostly Accessible	Fowler Ave	N	2	No	No	No	No
Mostly Accessible	Fowler Ave	N	2	No	No	No	No
Mostly Accessible	McNaughton Ave	N	2	No	No	No	No
Mostly Accessible	Central Ave	S	2	No	No	Yes	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible	Central Ave	S	2	No	No	No	No
Mostly Accessible	Forbes Ave	W	2	No	No	No	No
Mostly Accessible	Broadway	E	2	No	No	Yes	No
Mostly Accessible	First St	W	2	No	No	No	No
Mostly Accessible	Central Ave	N	2	No	No	No	No
Mostly Accessible	Chestnut St	S	2	No	No	No	No
Mostly Accessible	Eighth St	E	2	No	No	No	No
Mostly Accessible	Tenth St	W	2	No	No	No	No
Mostly Accessible	Tenth St	W	2	No	No	No	No
Mostly Accessible	Bellview Ter	E	2	No	No	No	No
Mostly Accessible	Second St	E	2	No	No	No	No
Mostly Accessible	Patten Ave	N	2	No	No	No	No
Mostly Accessible	Patten Ave	N	2	No	No	No	No
Mostly Accessible	Forbes Ave	E	2	No	No	No	No
Mostly Accessible	St Anthony La	N	2	No	No	No	No
Mostly Accessible	St Anthony La	E	2	No	No	No	No
Mostly Accessible	Bishop Hubbard Cir	S	2	No	No	No	No
Mostly Accessible	Bishop Hubbard Cir	W	2	No	No	No	No
Mostly Accessible	St Anthony La	N	2	No	No	No	No
Mostly Accessible	St Anthony La	S	2	No	No	No	No
Mostly Accessible	St Anthony La	W	2	No	No	No	No
Mostly Accessible	St Anthony La	N	2	No	No	No	No
Mostly Accessible	Green St	W	2	No	No	No	No
Mostly Accessible	Riverside Ave	W	2	No	No	No	No
Mostly Accessible	South St	E	3	No	No	No	No
Mostly Accessible	South St	E	3	No	No	No	No
Mostly Accessible	Rensselaer Ave	N	3	No	No	No	No
Mostly Accessible	Columbia St	W	3	No	No	No	No
Mostly Accessible	Riverside Ave	E	3	No	No	No	No
Mostly Accessible	Rensselaer Ave	S	3	No	No	No	No
Mostly Accessible	Green St	E	3	No	No	No	No
Mostly Accessible	Second Ave	S	3	No	No	Yes	No
Mostly Accessible	Second Ave	S	3	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible	Green St	W	3	No	No	No	No
Mostly Accessible	Green St	W	3	No	No	No	No
Mostly Accessible	Green St	W	3	No	No	No	No
Mostly Accessible	Green St	E	3	No	No	No	No
Mostly Accessible	Green St	E	3	No	No	No	No
Mostly Accessible	Aiken Ave	S	3	No	No	No	No
Mostly Accessible	Seventh St	W	3	No	No	No	No
Mostly Accessible	Joseph P Mink Dr	E	3	No	No	No	No
Mostly Accessible	Joseph P Mink Dr	W	3	No	No	No	No
Mostly Accessible	Spruce St	E	3	No	No	No	No
Mostly Accessible	Spruce St	W	3	No	No	No	No
Mostly Accessible	Aiken Ave	S	3	No	No	No	No
Mostly Accessible	Aiken Ave	S	3	No	No	No	No
Mostly Accessible	Second Ave	N	3	No	No	No	No
Mostly Accessible	Dubuque St	S	3	No	No	No	No
Mostly Accessible	Summit St	W	3	No	No	No	No
Mostly Accessible	Summit St	E	3	No	No	No	No
Mostly Accessible	Partition St	S	3	No	No	No	No
Mostly Accessible	Partition St	S	3	No	No	No	No
Mostly Accessible	Partition St	N	3	No	No	No	No
Mostly Accessible	Partition St	N	3	No	No	No	No
Mostly Accessible	Cottage Hill St	W	3	No	No	No	No
Mostly Accessible	Sixth St	W	3	No	No	No	No
Mostly Accessible	Seventh St	W	3	No	No	No	No
Mostly Accessible	Sixth St	W	3	No	No	No	No
Mostly Accessible	Sixth St	W	3	No	No	No	No
Mostly Accessible	Sixth St	W	3	No	No	No	No
Mostly Accessible	Sixth St	E	3	No	No	No	No
Mostly Accessible	Fifth St	E	3	No	No	No	No
Mostly Accessible	Fifth St	E	3	No	No	No	No
Mostly Accessible	Fifth St	E	3	No	No	No	No
Mostly Accessible	Fifth St	E	3	No	No	No	No
Mostly Accessible	Fifth St	W	3	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible	Fifth St	W	3	No	No	No	No
Mostly Accessible	Fifth St	W	3	No	No	No	No
Mostly Accessible	First St	E	3	No	No	No	No
Mostly Accessible	Second St	E	3	No	No	Yes	No
Mostly Accessible	First St	E	3	No	No	No	No
Mostly Accessible	Second St	W	3	No	No	No	No
Mostly Accessible	Fourth St	W	3	No	No	No	No
Mostly Accessible	Fourth St	W	3	No	No	No	No
Mostly Accessible	First St	E	3	No	No	No	No
Mostly Accessible	First St	E	3	No	No	No	No
Mostly Accessible	Second St	W	3	No	No	No	No
Mostly Accessible	Second St	W	3	No	No	No	No
Mostly Accessible	Second St	W	3	No	No	No	No
Mostly Accessible	Second St	E	3	No	No	No	No
Mostly Accessible	Fourth St	W	3	No	No	No	No
Mostly Accessible	Pine St	S	3	No	No	No	No
Mostly Accessible	Pine St	S	3	No	No	No	No
Mostly Accessible	Pine St	S	3	No	No	No	No
Mostly Accessible	Second St	W	3	No	No	No	No
Mostly Accessible	Second St	W	3	No	No	No	No
Mostly Accessible	Pine St	N	3	No	No	No	No
Mostly Accessible	Pine St	N	3	No	No	No	No
Mostly Accessible	Pine St	N	3	No	No	No	No
Mostly Accessible	Fourth St	E	3	No	No	No	No
Mostly Accessible	Pine St	N	3	No	No	No	No
Mostly Accessible	Fourth St	W	3	No	No	No	No
Mostly Accessible	Central Ave	N	3	No	No	No	No
Mostly Accessible	Fourth St	W	3	No	No	No	No
Mostly Accessible	Central Ave	S	3	No	No	No	No
Mostly Accessible	Dale St	N	3	No	No	No	Yes
Mostly Accessible	Fifth St	E	3	No	No	No	No
Mostly Accessible	Sixth St	W	3	No	No	No	No
Mostly Accessible	Seventh St	W	3	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible	Chestnut St	S	3	No	No	No	No
Mostly Accessible	Seventh St	E	3	No	No	No	No
Mostly Accessible	Eighth St	W	3	No	No	No	No
Mostly Accessible	Seventh St	E	3	No	No	No	No
Mostly Accessible	Seventh St	E	3	No	No	No	No
Mostly Accessible	Eighth St	W	3	No	No	No	No
Mostly Accessible	Ninth St	E	3	No	No	No	No
Mostly Accessible	Ninth St	W	3	No	No	No	No
Mostly Accessible	Tenth St	W	3	No	No	No	No
Mostly Accessible	Tenth St	W	3	No	No	No	No
Mostly Accessible	Cedar Run	W	3	No	No	No	No
Mostly Accessible	Valley View Blvd	N	3	No	No	No	No
Mostly Accessible	Valley View Blvd	N	3	No	No	No	No
Mostly Accessible	Valley View Blvd	N	3	No	No	No	No
Mostly Accessible	Valley View Blvd	N	3	No	No	No	No
Mostly Accessible	Valley View Blvd	S	3	No	No	No	No
Mostly Accessible	Valley View Blvd	W	3	No	No	No	No
Mostly Accessible	Valley View Blvd	W	3	No	No	No	No
Mostly Accessible	Valley View Blvd	W	3	No	No	No	No
Mostly Accessible	Valley View Blvd	W	3	No	No	No	No
Mostly Accessible	Valley View Blvd	W	3	No	No	No	No
Mostly Accessible	Valley View Blvd	W	3	No	No	No	No
Mostly Accessible	Amalia La	W	3	No	No	No	No
Mostly Accessible	Thornberry La	S	3	No	No	No	No
Mostly Accessible	First St	E	3	No	No	No	No
Mostly Accessible	Pine St	N	3	No	No	Yes	No
Mostly Accessible	Scott Pl	N	3	No	No	No	No
Mostly Accessible	Northside Ave	N	3	No	No	No	No
Mostly Accessible	Northside Ave	N	3	No	No	No	No
Mostly Accessible	Amalia La	N	3	No	No	No	No
Mostly Accessible	Amalia La	N	3	No	No	No	No
Mostly Accessible	Third Ave	N	3	No	No	No	No
Mostly Accessible	South St	W	4	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible	River Rd Ramp to US 9	S	4	No	No	No	No
Mostly Accessible	Columbia St	E	4	No	No	No	No
Mostly Accessible	South St	W	4	No	No	No	No
Mostly Accessible	NY 9J Ramp to US 9/U	N	4	No	No	No	No
Mostly Accessible	Columbia St	E	4	No	No	No	No
Mostly Accessible	Columbia St	W	4	No	No	No	No
Mostly Accessible	New Broadway	E	4	No	No	Yes	No
Mostly Accessible	Broadway	N	4	No	No	No	No
Mostly Accessible	Columbia St	E	4	No	No	No	No
Mostly Accessible	South St	E	5	No	No	Yes	No
Mostly Accessible	South St	E	5	No	No	No	No
Mostly Accessible	South St	E	5	No	No	No	No
Mostly Accessible	Thornberry La	S	5	No	No	No	No
Mostly Accessible	Thornberry La	S	5	No	No	No	No
Mostly Accessible	Thornberry La	S	5	No	No	No	No
Mostly Accessible	Thornberry La	S	5	No	No	No	No
Mostly Accessible	Thornberry La	S	5	No	No	No	No
Mostly Accessible	Thornberry La	N	5	No	No	No	No
Mostly Accessible	Overlook Dr	E	5	No	No	No	No
Mostly Accessible	River Chase Dr	N	5	No	No	No	No
Mostly Accessible	Cedar Run	E	5	No	No	No	No
Mostly Accessible	Cedar Run	S	5	No	No	No	No
Fully Accessible	Columbia St	E	1	No	No	No	No
Fully Accessible	Columbia St	E	1	No	No	No	No
Fully Accessible	Aiken Ave	N	1	No	No	No	No
Fully Accessible	Columbia St	E	1	No	No	No	No
Fully Accessible	Columbia St	W	1	No	No	No	No
Fully Accessible	Washington St	E	1	No	No	No	No
Fully Accessible	Third Ave	S	1	No	No	No	No
Fully Accessible	East St	W	1	No	No	No	No
Fully Accessible	East St	W	1	No	No	No	No
Fully Accessible	Herrick St	N	1	No	No	No	No
Fully Accessible	East St	W	1	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Fully Accessible	East St	W	1	No	No	No	No
Fully Accessible	Third Ave	N	1	No	No	No	No
Fully Accessible	Third Ave	N	1	No	No	No	No
Fully Accessible	Broadway	W	1	No	No	No	No
Fully Accessible	Fourth Ave	S	1	No	No	No	No
Fully Accessible	Broadway	W	1	No	No	No	No
Fully Accessible	Broadway	W	1	No	No	No	No
Fully Accessible	Herrick St	S	1	No	No	No	No
Fully Accessible	Huyck Sq	S	1	No	No	No	No
Fully Accessible	Huyck Sq	S	1	No	No	No	No
Fully Accessible	Third St	W	1	No	No	No	No
Fully Accessible	Washington Ave	S	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	S	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	S	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Washington Ave	N	1	No	No	No	No
Fully Accessible	Adams St	W	1	No	No	No	No
Fully Accessible	Forbes Ave	E	1	No	No	No	No
Fully Accessible	Aiken Ave	N	2	No	No	No	No
Fully Accessible	1st Alley	E	2	No	No	No	No
Fully Accessible	Green St	E	2	No	No	No	No
Fully Accessible	Washington St	E	2	No	No	No	No
Fully Accessible	1st Alley	E	2	No	No	No	No
Fully Accessible	Belmore Pl	N	3	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Fully Accessible	Aiken Ave	S	3	No	No	No	No
Fully Accessible	Sixth St	W	3	No	No	No	No
Fully Accessible	Fifth St	E	3	No	No	No	No
Fully Accessible	First St	E	3	No	No	No	No
Fully Accessible	Pine St	S	3	No	No	No	No
Fully Accessible	Sixth St	W	3	No	No	No	No
Fully Accessible	Sixth St	W	3	No	No	No	No
Fully Accessible	Sixth St	W	3	No	No	No	No
Fully Accessible	Dale St	S	3	No	No	No	No
Fully Accessible	Dale St	S	3	No	No	No	No
Fully Accessible	Sixth St	E	3	No	No	No	No
Fully Accessible	Sixth St	E	3	No	No	No	No
Fully Accessible	Sixth St	E	3	No	No	No	No
Fully Accessible	Seventh St	W	3	No	No	No	No
Fully Accessible	Dale St	N	3	No	No	No	No
Fully Accessible	Sixth St	E	3	No	No	No	No
Fully Accessible	Eighth St	E	3	No	No	No	No
Fully Accessible	Ninth St	W	3	No	No	Yes	No
Fully Accessible	Eighth St	W	3	No	No	No	No
Fully Accessible	Eighth St	W	3	No	No	No	No
Fully Accessible	Tenth St	W	3	No	No	No	No
Fully Accessible	Thornberry La	S	3	No	No	No	No
Fully Accessible	Station Rd	W	5	No	No	No	No
Fully Accessible	Station Rd	W	5	No	No	No	No
Fully Accessible			5	No	No	No	No

Intersections

Accessibility Barrier	Road Name	Street Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
Multiple Intersection Barrier	Eighth St	W	1	Yes	No	Yes	Yes	Yes	No	Yes	No
Multiple Intersection Barrier	Broadway	E	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No
Multiple Intersection Barrier	Belmore Pl	N	1	Yes	Yes	No	Yes	No	Yes	No	No
Multiple Intersection Barrier	Bellview Ter	E	2	Yes	No	Yes	Yes	Yes	Yes	No	Yes
Multiple Intersection Barrier	First St	E	2	Yes	No	No	Yes	Yes	Yes	No	No
Multiple Intersection Barrier	Broadway	E	2	Yes	No	Yes	Yes	Yes	Yes	Yes	No
Multiple Intersection Barrier	Second Ave	N	2	Yes	No	No	Yes	No	Yes	No	Yes
Multiple Intersection Barrier	First Ave	N	2	Yes	No	Yes	Yes	No	Yes	No	Yes
Multiple Intersection Barrier	Fifth St	W	3	Yes	No	No	Yes	No	No	Yes	Yes
Inaccessible Curb Ramp	Washington S	W	1	Yes	No	Yes	No				
Inaccessible Curb Ramp	Tracy St	N	1	Yes	No	Yes	No				
Inaccessible Curb Ramp	Forbes Ave	E	1	Yes	No	Yes	No				
Inaccessible Curb Ramp	Third St	E	1	Yes	No	Yes	No				
Inaccessible Curb Ramp	Aiken Ave	S	1	Yes	No	Yes	No				
Inaccessible Curb Ramp	Aiken Ave	N	1	Yes	No	Yes	No				
Inaccessible Curb Ramp	Fourth Ave	S	1	Yes	No	Yes	No				
Inaccessible Curb Ramp	Fourth Ave	S	2	Yes	No	Yes	No				
Inaccessible Curb Ramp	Fourth St	E	2	Yes	Yes	No	No				
Inaccessible Curb Ramp	Washington A	N	2	Yes	Yes	No	No				
Inaccessible Curb Ramp	Broadway	E	2	Yes	No	Yes	No				
Inaccessible Curb Ramp	Broadway	E	2	Yes	No	Yes	No				
Inaccessible Curb Ramp	First St	E	2	Yes	No	Yes	No				
Inaccessible Curb Ramp	Glen St	N	2	Yes	No	Yes	No				
Inaccessible Curb Ramp	First St	W	2	Yes	No	Yes	No				
Inaccessible Curb Ramp	First Ave	S	2	Yes	No	No	No				
Inaccessible Curb Ramp	Elmhurst Ave	E	3	Yes	No	No	No				
Inaccessible Curb Ramp	Ninth St	W	3	Yes	No	Yes	No				
Inaccessible Curb Ramp	Seventh St	E	3	Yes	No	No	No				
Inaccessible Curb Ramp	Seventh St	W	3	Yes	No	Yes	No				
Inaccessible Curb Ramp	Dale St	S	3	Yes	No	Yes	No				

Accessibility Barrier	Road Name	Street Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	East St	E	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	East St	W	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	East St	E	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Adams St	E	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Adams St	W	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Third Ave	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Huyck St	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	W	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Partition St	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Fourth Ave	N	2	Yes	Yes	Yes	No				
No Intersection Barriers	Washington S	W	2	Yes	Yes	Yes	No				
No Intersection Barriers	Fourth Ave	S	2	Yes	Yes	Yes	No				
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	S	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	S	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	S	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	S	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	I 90 On Ramp	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Tenth St	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Van Renssela	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Tenth St	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Tenth St	W	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Birch St	N	2	Yes	Yes	Yes	No				
No Intersection Barriers	Fourth St	E	2	Yes	Yes	Yes	No				
No Intersection Barriers	Fifth St	E	2	Yes	Yes	Yes	No				
No Intersection Barriers	Chestnut St	S	2	Yes	Yes	Yes	No				

Accessibility Barrier	Road Name	Street Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Central Ave	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Central Ave	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	First St	W	2	Yes	Yes	Yes	No				
No Intersection Barriers	First St	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Central Ave	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Tracy St	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	First St	W	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Fowler Ave	N	2	Yes	Yes	Yes	No				
No Intersection Barriers	Fowler Ave	N	2	Yes	Yes	Yes	No				
No Intersection Barriers	McNaughton /	S	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	McNaughton /	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	1st St	W	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	McNaughton /	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	1st St	W	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	McNaughton /	S	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	W	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	McNaughton /	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Catherine St	S	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Catherine St	S	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	First St	W	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	
No Intersection Barriers	Pine St	N	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	John St	N	2	Yes	Yes	Yes	No				
No Intersection Barriers	Broadway	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Third St	E	2	Yes	Yes	Yes	No				
No Intersection Barriers	Harrison Ave	N	2	Yes	Yes	Yes	No				

Accessibility Barrier	Road Name	Street Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Sixth St	W	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Partition St	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Cottage Hill S	W	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Partition St	S	3	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View B	S	3	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View B	S	3	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View B	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View B	W	3	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View B	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View B	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View B	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View B	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View B	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Northside Ave	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Northside Ave	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Scott Pl	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Scott Pl	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Washington A	N	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	
No Intersection Barriers	Washington A	N	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	S	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	S	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	S	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	N	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Washington A	N	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Tenth St	W	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Tenth St	W	3	Yes	Yes	Yes	No				
No Intersection Barriers	Ninth St	E	3	Yes	Yes	Yes	No				
No Intersection Barriers	Ninth St	E	3	Yes	Yes	Yes	No				
No Intersection Barriers	Ninth St	E	3	Yes	Yes	Yes	No				
No Intersection Barriers	Ninth St	W	3	Yes	Yes	Yes	No				
No Intersection Barriers	Birch St	S	3	Yes	Yes	Yes	No				

Accessibility Barrier	Road Name	Street Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Third Ave	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Third Ave	N	3	Yes	Yes	Yes	No				
No Intersection Barriers	Third Ave	S	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Aiken Ave	S	3	Yes	Yes	Yes	No				
No Intersection Barriers	Aiken Ave	S	3	Yes	Yes	Yes	No				
No Intersection Barriers	Spruce St	E	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Spruce St	W	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	3rd Ave	N	3	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	N	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Broadway	W	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Columbia St	E	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	9J	W	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	River Rd Ram	S	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	9J	W	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Route 9	E	4	Yes	Yes	Yes	No				
No Intersection Barriers	Columbia St	E	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Route 9	E	4	Yes	Yes	Yes	No				
No Intersection Barriers	South St	W	4	Yes	Yes	Yes	No				
No Intersection Barriers	South St	W	4	Yes	Yes	Yes	No				
No Intersection Barriers	South St	E	4	Yes	Yes	Yes	No				
No Intersection Barriers	South St	W	4	Yes	Yes	Yes	No				
No Intersection Barriers	River Chase Dr	N	5	Yes	Yes	Yes	No				
No Intersection Barriers	River Chase Dr	N	5	Yes	Yes	Yes	No				
No Intersection Barriers	River Chase Dr	N	5	Yes	Yes	Yes	No				
No Intersection Barriers	Thornberry La	S	5	Yes	Yes	Yes	No				
No Intersection Barriers	Thornberry La	S	5	Yes	Yes	Yes	No				
No Intersection Barriers	Thornberry La	S	5	Yes	Yes	Yes	No				
No Intersection Barriers	Thornberry La	S	5	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View Blvd	N	5	Yes	Yes	Yes	No				
No Intersection Barriers	Valley View Blvd	N	5	Yes	Yes	Yes	No				
No Intersection Barriers	St Anthony La	N	5	Yes	Yes	Yes	No				
No Intersection Barriers	St Anthony La	N	5	Yes	Yes	Yes	No				

Accessibility Barrier	Road Name	Street Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	St Anthony La	N	5	Yes	Yes	Yes	No				
No Intersection Barriers	St Anthony La	N	5	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Bishop Hubba	S	5	Yes	Yes	Yes	No				
No Intersection Barriers	St Francis Pl	N	5	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Aiken Ave	S	5	Yes	Yes	Yes	Yes		Yes	Yes	Yes
No Intersection Barriers	NY 9J Ramp	N	5	Yes	Yes	Yes	No				
No Curb Ramp	Lincoln Ter	N	1	No			No				
No Curb Ramp	Huyck Sq	N	1	No			No				
No Curb Ramp	Huyck Sq	S	1	No			No				
No Curb Ramp	Huyck Sq	S	1	No			No				
No Curb Ramp	Third Ave	S	1	No			No				
No Curb Ramp	Broadway	W	1	No			No				
No Curb Ramp	Broadway	W	1	No			No				
No Curb Ramp	Fourth Ave	N	1	No			No				
No Curb Ramp	Eighth St	E	2	No			No				
No Curb Ramp	Eighth St	E	2	No			No				
No Curb Ramp	Washington A	N	2	No			No				
No Curb Ramp	Washington A	S	2	No			No				
No Curb Ramp	First St	W	2	No			No				
No Curb Ramp	First St	W	2	No			No				
No Curb Ramp	Walker St	W	2	No			No				
No Curb Ramp	Second Ave	S	2	No			No				
No Curb Ramp	Fifth St	E	3	No			No				
No Curb Ramp	Fifth St	W	3	No			No				
No Curb Ramp	Partition St	N	3	No			No				
No Curb Ramp	Ninth St	W	3	No			No				
No Curb Ramp	Birch St	N	3	No			No				
No Curb Ramp	Eighth St	W	3	No			No				
No Curb Ramp	7th St	W	3	No			No				
No Curb Ramp	Dale St	N	3	No			No				
No Curb Ramp	Dale St	S	3	No			No				
No Curb Ramp	Dale St	S	3	No			No				

Accessibility Barrier	Road Name	Street Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Curb Ramp	Fifth St	E	3	No			No				
No Curb Ramp	Dale St	N	3	No			No				
No Curb Ramp	Dale St	S	3	No			No				
No Curb Ramp	Central Ave	N	3	No			No				
No Curb Ramp	Central Ave	S	3	No			No				
No Curb Ramp	Central Ave	S	3	No			No				
No Curb Ramp	Central Ave	S	3	No			No				
No Curb Ramp	Washington A	N	3	No			No				
No Curb Ramp	Fowler Ave	N	3	No			No				
No Curb Ramp	Fowler Ave	S	3	No			No				
No Curb Ramp	Catherine St	N	3	No			No				
No Curb Ramp	Second St	E	3	No			No				
No Curb Ramp	Second St	E	3	No			No				
No Curb Ramp	Second St	W	3	No			No				
No Curb Ramp	First St	E	3	No			No				
No Curb Ramp	John St	N	3	No			No				
No Curb Ramp	Fourth St	E	3	No			No				
No Curb Ramp	John St	S	3	No			No				
No Curb Ramp	Wendell St	N	3	No			No				
No Curb Ramp	Lawrence St	W	3	No			No				
No Curb Ramp	Elm St	E	3	No			No				
No Curb Ramp	Dubuque St	S	3	No			No				
No Curb Ramp	Green St	W	3	No			No				
No Curb Ramp	2nd Ave	S	3	No			No				
No Curb Ramp	2nd Ave	S	3	No			No				
No Curb Ramp	Riverside Ave	E	3	No			No				
No Curb Ramp	Lawrence St	E	3	No			No				
No Curb Ramp	Third Ave	N	3	No			No				
No Curb Ramp	Aiken Ave	S	3	No			No				
No Curb Ramp	New Broadwa	W	4	No			No				
No Curb Ramp	St Francis Pl	N	5	No			No				
No Curb Ramp	Second Ave	S	5	No			No				

Crossings

Accessibility Rating	Road Name	Priority	Pedestrian access route?	Deterioration Type	Designated crosswalk?	Crosswalk Condition?
Crossing Barrier	2nd Street	1	Yes	Cracks/Potholes	No	
Crossing Barrier	3rd Street	1	Yes	Cracks/Potholes	Yes	Poor
Crossing Barrier	3rd Street	1	Yes	Cracks/Potholes	No	
Crossing Barrier	Academy Street	1	Yes	Cracks/Potholes	Yes	Good
Crossing Barrier	3rd Avenue	1	No		No	
Crossing Barrier	Cambridge Avenue	1	Yes	Cracks/Potholes	No	
Crossing Barrier	Broadway	1	No		No	
Crossing Barrier	Saint Anthony Ln	2	No		No	
Crossing Barrier	Birch Street	2	No		No	
Crossing Barrier	Patten Avenue	2	Yes	Cracks/Potholes	No	
Crossing Barrier	Chestnut Street	2	Yes	Cracks/Potholes	No	
Crossing Barrier	1st Street	2	Yes	Improper cross-slope	No	
Crossing Barrier	1st Street	2	No		No	
Crossing Barrier	Broadway	2	Yes	Cracks/Potholes	No	
Crossing Barrier	1st Street	2	No		No	
Crossing Barrier	1st Street	2	No		No	
Crossing Barrier	1st Street	2	No		No	
Crossing Barrier	Academy Street	2	Yes	Cracks/Potholes	No	
Crossing Barrier	Walker Street	2	No		No	
Crossing Barrier	1st Avenue	2	No		No	
Crossing Barrier	Green Street	2	Yes	Cracks/Potholes	No	
Crossing Barrier	Valley View Boulevard	3	Yes		No	
Crossing Barrier	Valley View Boulevard	3	Yes		No	
Crossing Barrier	Munger Street	3	Yes	Cracks/Potholes	No	
Crossing Barrier	8th Street	3	Yes		No	
Crossing Barrier	8th Street	3	Yes	Cracks/Potholes	No	
Crossing Barrier	Birch Street	3	No		No	
Crossing Barrier	5th Street	3	No		No	
Crossing Barrier	6th Street	3	Yes	Cracks/Potholes	No	
Crossing Barrier	4th Street	3	No		No	

Crossing Barrier	4th Street	3	Yes	Cracks/Potholes	No	
Crossing Barrier	Catherine Street	3	Yes	Cracks/Potholes	No	
Crossing Barrier	4th Street	3	Yes	Parked vehicle(s)	No	
Crossing Barrier	Glen Street	3	No		No	
Crossing Barrier	John Street	3	No		No	
Crossing Barrier	John Street	3	Yes	Cracks/Potholes	No	
Crossing Barrier	2nd Street	3	No		No	
Crossing Barrier	4th Street	3	Yes	Cracks/Potholes	No	
Crossing Barrier	Green Street	3	No		No	
Crossing Barrier	Belmore Place	3	Yes	Cracks/Potholes	Yes	Fair
No Crossing Barrier	Washington Street	1	Yes		Yes	Good
No Crossing Barrier	Washington Street	1	Yes		Yes	Good
No Crossing Barrier	Washington Street	1	Yes		Yes	Fair
No Crossing Barrier	Washington Street	1	Yes		Yes	Good
No Crossing Barrier	Unnamed Path	1	Yes		No	
No Crossing Barrier	Lindbergh Avenue	1	Yes		No	
No Crossing Barrier	Woodland Avenue	1	Yes		No	
No Crossing Barrier	Woodland Avenue	1	Yes		No	
No Crossing Barrier	Birchwood Avenue	1	Yes		No	
No Crossing Barrier	Birchwood Avenue	1	Yes		No	
No Crossing Barrier	Elmhurst Avenue	1	Yes		No	
No Crossing Barrier	Washington Avenue	1	Yes		No	
No Crossing Barrier	Washington Avenue	1	Yes		No	
No Crossing Barrier	Delaware Avenue	1	Yes		No	
No Crossing Barrier	Washington Avenue	1	Yes		No	
No Crossing Barrier	Quay Street	1	Yes		No	
No Crossing Barrier	Washington Avenue	1	Yes		No	
No Crossing Barrier	Washington Avenue	1	Yes		No	
No Crossing Barrier	Rockefeller Street	1	Yes		No	
No Crossing Barrier	St Francis	1	Yes		No	
No Crossing Barrier	Ramp	1	Yes		Yes	Good
No Crossing Barrier	Ramp	1	Yes		Yes	Fair
No Crossing Barrier	Old Washington Avenue	1	Yes		No	
No Crossing Barrier	Old Washington Avenue	1	Yes		No	

No Crossing Barrier	Washington Avenue	1	Yes		Yes	Good
No Crossing Barrier	Washington Avenue	1	Yes		No	
No Crossing Barrier	Washington Avenue	1	Yes		Yes	Good
No Crossing Barrier	Washington Avenue	1	Yes		Yes	Good
No Crossing Barrier	Washington Avenue	1	Yes		Yes	Good
No Crossing Barrier	Washington Avenue	1	Yes		Yes	Good
No Crossing Barrier	Washington Avenue	1	Yes		Yes	Good
No Crossing Barrier	Forbes Avenue	1	Yes		Yes	Poor
No Crossing Barrier	Washington Avenue	1	Yes		Yes	Fair
No Crossing Barrier	Washington Avenue	1	Yes		Yes	Poor
No Crossing Barrier	Washington Avenue	1	Yes		Yes	Poor
No Crossing Barrier	Broadway	1	Yes		No	
No Crossing Barrier	Broadway	1	Yes		No	
No Crossing Barrier	Broadway	1	Yes		Yes	Poor
No Crossing Barrier	Broadway	1	Yes		Yes	Poor
No Crossing Barrier	Forbes Avenue	1	Yes		No	
No Crossing Barrier	2nd Street	1	Yes		No	
No Crossing Barrier	2nd Street	1	Yes		No	
No Crossing Barrier	3rd Street	1	Yes		No	
No Crossing Barrier	3rd Street	1	Yes		Yes	Fair
No Crossing Barrier	Fowler Avenue	1	Yes		Yes	Poor
No Crossing Barrier	Fowler Avenue	1	Yes		Yes	Poor
No Crossing Barrier	Fowler Avenue	1	Yes		Yes	Poor
No Crossing Barrier	Fowler Avenue	1	Yes		Yes	Poor
No Crossing Barrier	3rd Street	1	Yes		No	
No Crossing Barrier	3rd Street	1	Yes		No	
No Crossing Barrier	Catherine Street	1	Yes		Yes	Excellent
No Crossing Barrier	Catherine Street	1	Yes		Yes	Excellent
No Crossing Barrier	Catherine Street	1	Yes		Yes	Excellent
No Crossing Barrier	Catherine Street	1	Yes		Yes	Excellent
No Crossing Barrier	3rd Street	1	Yes		Yes	Poor
No Crossing Barrier	3rd Street	1	Yes		Yes	Poor
No Crossing Barrier	3rd Street	1	Yes		Yes	Poor
No Crossing Barrier	3rd Street	1	Yes		Yes	Poor

No Crossing Barrier	3rd Street	1	Yes		No	
No Crossing Barrier	3rd Street	1	Yes		No	
No Crossing Barrier	1st Street	1	Yes		Yes	Good
No Crossing Barrier	1st Street	1	Yes		Yes	Good
No Crossing Barrier	1st Street	1	Yes		Yes	Good
No Crossing Barrier	1st Street	1	Yes		Yes	Good
No Crossing Barrier	Willow Street	1	Yes		Yes	Good
No Crossing Barrier	Willow Street	1	Yes		Yes	Excellent
No Crossing Barrier	Willow Street	1	Yes		Yes	Good
No Crossing Barrier	Elm Street	1	Yes		Yes	Good
No Crossing Barrier	Elm Street	1	Yes		Yes	Good
No Crossing Barrier	Elm Street	1	Yes		Yes	Good
No Crossing Barrier	Elm Street	1	Yes		Yes	Good
No Crossing Barrier	Elm Street	1	Yes		Yes	Good
No Crossing Barrier	East Street	1	Yes		Yes	Excellent
No Crossing Barrier	East Street	1	Yes		Yes	Good
No Crossing Barrier	East Street	1	Yes		Yes	Good
No Crossing Barrier	East Street	1	Yes		Yes	Good
No Crossing Barrier	Wendell Street	1	Yes		Yes	Good
No Crossing Barrier	Broadway	1	Yes		Yes	Good
No Crossing Barrier	Broadway	1	Yes		Yes	Poor
No Crossing Barrier	Broadway	1	Yes		Yes	Fair
No Crossing Barrier	Broadway	1	Yes		Yes	Fair
No Crossing Barrier	4th Avenue	1	Yes		Yes	Good
No Crossing Barrier	Huyck Square	1	Yes		Yes	Good
No Crossing Barrier	Driveway	1	Yes		No	
No Crossing Barrier	Broadway	1	Yes		Yes	Fair
No Crossing Barrier	Broadway	1	Yes		Yes	Good
No Crossing Barrier	Broadway	1	Yes		Yes	Good
No Crossing Barrier	Broadway	1	Yes		Yes	Good
No Crossing Barrier	Broadway	1	Yes		Yes	Fair
No Crossing Barrier	Academy Street	1	Yes		Yes	Good
No Crossing Barrier	Driveway	1	Yes		No	
No Crossing Barrier	3rd Avenue	1	Yes		No	
No Crossing Barrier	Parking Lot	1	Yes		Yes	Good

No Crossing Barrier	Washington Street	1	Yes		Yes	Good
No Crossing Barrier	Academy Street	1	Yes		Yes	Good
No Crossing Barrier	Academy Street	1	Yes		Yes	Good
No Crossing Barrier	Broadway	1	Yes		Yes	Good
No Crossing Barrier	Broadway	1	Yes		Yes	Excellent
No Crossing Barrier	Broadway	1	Yes		Yes	Good
No Crossing Barrier	Columbia Street	1	Yes		Yes	Good
No Crossing Barrier	Columbia Street	1	Yes		Yes	Good
No Crossing Barrier	Columbia Street	1	Yes		Yes	Good
No Crossing Barrier	Aiken Avenue	1	Yes		Yes	Good
No Crossing Barrier	Columbia Street	1	Yes		Yes	Good
No Crossing Barrier	Mansion Ave	1	Yes		Yes	Poor
No Crossing Barrier	4th Avenue	1	Yes		Yes	Good
No Crossing Barrier	4th Avenue	1	Yes		Yes	Good
No Crossing Barrier	Adams Street	1	Yes		Yes	Good
No Crossing Barrier	Adams Street	1	Yes		Yes	Good
No Crossing Barrier	3rd Avenue	1	Yes		Yes	Good
No Crossing Barrier	3rd Avenue	1	Yes		Yes	Good
No Crossing Barrier	Broadway	1	Yes		Yes	Fair
No Crossing Barrier	Washington Street	2	Yes		Yes	Fair
No Crossing Barrier	Washington Street	2	Yes		Yes	Poor
No Crossing Barrier	7th St	2	Yes		Yes	Fair
No Crossing Barrier	Broadway	2	Yes		Yes	Fair
No Crossing Barrier	Broadway	2	Yes		Yes	Poor
No Crossing Barrier	Washington Avenue	2	Yes		No	
No Crossing Barrier	Washington Avenue	2	Yes		No	
No Crossing Barrier	Farley Drive	2	Yes		No	
No Crossing Barrier	Ramp	2	Yes		Yes	Good
No Crossing Barrier	Ramp	2	Yes		Yes	Good
No Crossing Barrier	St Francis	2	Yes		No	
No Crossing Barrier	10th Street	2	Yes		Yes	Poor
No Crossing Barrier	10th Street	2	Yes		Yes	Fair
No Crossing Barrier	Ash Street	2	Yes		No	
No Crossing Barrier	3rd Street	2	Yes		Yes	Poor

No Crossing Barrier	3rd Street	2	Yes		No	
No Crossing Barrier	3rd Street	2	Yes		No	
No Crossing Barrier	3rd Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	Broadway	2	Yes		No	
No Crossing Barrier	Bellview Terrace	2	Yes		No	
No Crossing Barrier	Bellview Terrace	2	Yes		No	
No Crossing Barrier	Bellview Terrace	2	Yes		No	
No Crossing Barrier	Mcnaughton Avenue	2	Yes		No	
No Crossing Barrier	Mcnaughton Avenue	2	Yes		No	
No Crossing Barrier	Mcnaughton Avenue	2	Yes		No	
No Crossing Barrier	Broadway	2	Yes		Yes	Good
No Crossing Barrier	Broadway	2	Yes		Yes	Good
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	Broadway	2	Yes		Yes	Excellent
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	Glen Street	2	Yes		No	
No Crossing Barrier	1st Street	2	Yes		No	
No Crossing Barrier	Broadway	2	Yes		No	
No Crossing Barrier	3rd Street	2	Yes		No	

No Crossing Barrier	3rd Street	2	Yes		No	
No Crossing Barrier	3rd Street	2	Yes		No	
No Crossing Barrier	3rd Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		No	
No Crossing Barrier	Harrison Avenue	2	Yes		No	
No Crossing Barrier	Harrison Avenue	2	Yes		No	
No Crossing Barrier	Harrison Avenue	2	Yes		No	
No Crossing Barrier	Broadway	2	Yes		No	
No Crossing Barrier	2nd Street	2	Yes		Yes	Good
No Crossing Barrier	2nd Street	2	Yes		Yes	Good
No Crossing Barrier	2nd Street	2	Yes		Yes	Fair
No Crossing Barrier	Broadway	2	Yes		Yes	Good
No Crossing Barrier	Broadway	2	Yes		Yes	Good
No Crossing Barrier	Lawrence Street	2	Yes		Yes	Good
No Crossing Barrier	Herrick Street	2	Yes		Yes	Good
No Crossing Barrier	Broadway	2	Yes		Yes	Good
No Crossing Barrier	Broadway	2	Yes		Yes	Fair
No Crossing Barrier	Broadway	2	Yes		Yes	Fair
No Crossing Barrier	Huyck Square	2	Yes		No	
No Crossing Barrier	Broadway	2	Yes		Yes	Good
No Crossing Barrier	2nd Avenue	2	Yes		Yes	Good
No Crossing Barrier	2nd Avenue	2	Yes		Yes	Good
No Crossing Barrier	2nd Avenue	2	Yes		Yes	Good
No Crossing Barrier	2nd Avenue	2	Yes		Yes	Good
No Crossing Barrier	Academy Street	2	Yes		No	
No Crossing Barrier	Washington Street	2	Yes		No	
No Crossing Barrier	Washington Street	2	Yes		No	
No Crossing Barrier	Walker Street	2	Yes		No	
No Crossing Barrier	Green Street	2	Yes		No	
No Crossing Barrier	Green Street	2	Yes		No	
No Crossing Barrier	1st Avenue	2	Yes		No	

No Crossing Barrier	Washington Street	2	Yes		Yes	Poor
No Crossing Barrier	Academy Street	2	Yes		No	
No Crossing Barrier	Aiken Avenue	2	Yes		Yes	Fair
No Crossing Barrier	Columbia Street	2	Yes		Yes	Poor
No Crossing Barrier	Parking Lot	2	Yes		Yes	Poor
No Crossing Barrier	East Street	2	Yes		Yes	Good
No Crossing Barrier	6th Street	3	Yes		No	
No Crossing Barrier	6th Street	3	Yes		No	
No Crossing Barrier	6th Street	3	Yes		No	
No Crossing Barrier	Aiken Avenue	3	Yes		No	
No Crossing Barrier	Valley View Boulevard	3	Yes		No	
No Crossing Barrier	Evergreen St	3	Yes		Yes	Good
No Crossing Barrier	Evergreen Pl	3	Yes		Yes	Good
No Crossing Barrier	Oak	3	Yes		No	
No Crossing Barrier	Elmhurst	3	Yes		No	
No Crossing Barrier	Washington Avenue	3	Yes		No	
No Crossing Barrier	Lakeview Avenue	3	Yes		No	
No Crossing Barrier	10th Street	3	Yes		No	
No Crossing Barrier	Ash Street	3	Yes		No	
No Crossing Barrier	Ash Street	3	Yes		No	
No Crossing Barrier	Ash Street	3	Yes		No	
No Crossing Barrier	Ash Street	3	Yes		No	
No Crossing Barrier	9th Street	3	Yes		No	
No Crossing Barrier	9th Street	3	Yes		No	
No Crossing Barrier	8th Street	3	Yes		No	
No Crossing Barrier	8th Street	3	Yes		No	
No Crossing Barrier	7th Street	3	Yes		No	
No Crossing Barrier	7th Street	3	Yes		No	
No Crossing Barrier	7th Street	3	Yes		No	
No Crossing Barrier	Chestnut Street	3	Yes		No	
No Crossing Barrier	7th Street	3	Yes		No	
No Crossing Barrier	7th Street	3	Yes		No	
No Crossing Barrier	6th Street	3	Yes		No	
No Crossing Barrier	6th Street	3	Yes		No	

No Crossing Barrier	6th Street	3	Yes		No	
No Crossing Barrier	6th Street	3	Yes		No	
No Crossing Barrier	5th Street	3	Yes		No	
No Crossing Barrier	5th Street	3	Yes		No	
No Crossing Barrier	5th Street	3	Yes		No	
No Crossing Barrier	Dale Street	3	Yes		Yes	Good
No Crossing Barrier	6th Street	3	Yes		No	
No Crossing Barrier	6th Street	3	Yes		No	
No Crossing Barrier	6th Street	3	Yes		No	
No Crossing Barrier	5th Street	3	Yes		No	
No Crossing Barrier	5th Street	3	Yes		No	
No Crossing Barrier	5th Street	3	Yes		No	
No Crossing Barrier	5th Street	3	Yes		No	
No Crossing Barrier	4th Street	3	Yes		No	
No Crossing Barrier	4th Street	3	Yes		No	
No Crossing Barrier	4th Street	3	Yes		No	
No Crossing Barrier	4th Street	3	Yes		No	
No Crossing Barrier	2nd Street	3	Yes		No	
No Crossing Barrier	4th Street	3	Yes		No	
No Crossing Barrier	3rd Street	3	Yes		Yes	Poor
No Crossing Barrier	2nd Street	3	Yes		No	
No Crossing Barrier	2nd Street	3	Yes		No	
No Crossing Barrier	2nd Street	3	Yes		No	
No Crossing Barrier	Catherine Street	3	Yes		No	
No Crossing Barrier	Catherine Street	3	Yes		Yes	Good
No Crossing Barrier	4th Street	3	Yes		No	
No Crossing Barrier	4th Street	3	Yes		No	
No Crossing Barrier	4th Street	3	Yes		No	
No Crossing Barrier	4th Street	3	Yes		No	
No Crossing Barrier	3rd Street	3	Yes		Yes	Poor
No Crossing Barrier	3rd Street	3	Yes		No	
No Crossing Barrier	3rd Street	3	Yes		No	
No Crossing Barrier	3rd Street	3	Yes		No	
No Crossing Barrier	2nd Street	3	Yes		No	

No Crossing Barrier	2nd Street	3	Yes		No	
No Crossing Barrier	2nd Street	3	Yes		No	
No Crossing Barrier	2nd Street	3	Yes		No	
No Crossing Barrier	1st Street	3	Yes		No	
No Crossing Barrier	Glen Street	3	Yes		No	
No Crossing Barrier	Glen Street	3	Yes		No	
No Crossing Barrier	Glen Street	3	Yes		No	
No Crossing Barrier	1st Street	3	Yes		No	
No Crossing Barrier	John St	3	Yes		No	
No Crossing Barrier	John St	3	Yes		No	
No Crossing Barrier	2nd Street	3	Yes		No	
No Crossing Barrier	1st Street	3	Yes		No	
No Crossing Barrier	Harrison Avenue	3	Yes		No	
No Crossing Barrier	Partition Street	3	Yes		No	
No Crossing Barrier	Green Street	3	Yes		No	
No Crossing Barrier	Green Street	3	Yes		No	
No Crossing Barrier	Green Street	3	Yes		No	
No Crossing Barrier	Nelson Avenue	3	Yes		Yes	Poor
No Crossing Barrier	Columbia Street	3	Yes		Yes	Good
No Crossing Barrier	Plum Street	3	Yes		Yes	Excellent
No Crossing Barrier	3rd Avenue	3	Yes		Yes	Good
No Crossing Barrier	3rd Avenue	3	Yes		No	
No Crossing Barrier	Aiken Avenue	3	Yes		No	
No Crossing Barrier	Amtrak Way	4	Yes		Yes	Good
No Crossing Barrier	New Broadway	4	Yes		Yes	Good
No Crossing Barrier	South Street	4	Yes		Yes	Poor
No Crossing Barrier	Columbia Street	4	Yes		Yes	Good
No Crossing Barrier	Ramp	4	Yes		Yes	Good
No Crossing Barrier	9J	4	Yes		Yes	Good
No Crossing Barrier	Thornberry Ln	5				
No Crossing Barrier	Thornberry Ln	5	Yes		No	
No Crossing Barrier	Valley View Boulevard	5	Yes		No	

APPENDIX E: ADA POLICY STATEMENT

It is the policy of the City of Rensselaer that no person shall on the grounds of race, color, national origin, sex, disability, or age, be excluded from participation in, be denied the benefits of, or be subjected to discrimination in any operation of the City of Rensselaer as provided by Title VI of the Civil Rights Act of 1964 and related statutes.

This policy applies to all operations of the City of Rensselaer including its contractors and anyone who acts on behalf of the City of Rensselaer. This policy also applies to the operations of any department or agency to which the City of Rensselaer extends federal financial assistance. Federal financial assistance includes grants, training, use of equipment, donations of surplus property, and other assistance.

Prohibited discrimination may be intentional or unintentional. Seemingly neutral acts that have disparate impacts on individuals of a protected group and lack a substantial legitimate justification are a form of prohibited discrimination. Harassment and retaliation are also prohibited forms of discrimination.

Examples of prohibited types of discrimination based on race, color, national origin, sex, disability, or age include: Denial to an individual any service, financial aid, or other benefit; Distinctions in the quality, quantity, or manner in which a benefit is provided; Segregation or separate treatment; Restriction in the enjoyment of any advantages, privileges, or other benefits provided; Discrimination in any activities related to highway and infrastructure or facility built or repaired; and Discrimination in employment.

Title VI compliance is a condition of receipt of federal funds. The Title VI Coordinator is authorized to ensure compliance with this policy, Title VI of the Civil Rights Act of 1964, 42 U.S.C § 2000d and related statutes, and the requirements of 23 Code of Federal Regulation (CFR) pt. 200 and 49 CFR pt. 21.

NOTICE UNDER THE AMERICANS WITH DISABILITIES ACT

In accordance with the requirements of title II of the Americans with Disabilities Act of 1990 ("ADA"), the City of Rensselaer will not discriminate against qualified individuals with disabilities on the basis of disability in its services, programs, or activities.

Employment: The City of Rensselaer does not discriminate on the basis of disability in its hiring or employment practices and complies with all regulations promulgated by the U.S. Equal Employment Opportunity Commission under title I of the ADA.

Effective Communication: The City of Rensselaer will generally, upon request, provide appropriate aids and services leading to effective communication for qualified persons with disabilities so they can participate equally in the City of Rensselaer's programs, services,

and activities, including qualified sign language interpreters, documents in Braille, and other ways of making information and communications accessible to people who have speech, hearing, or vision impairments.

Modifications to Policies and Procedures: The City of Rensselaer will make all reasonable modifications to policies and programs to ensure that people with disabilities have an equal opportunity to enjoy all of its programs, services, and activities. For example, individuals with service animals are welcomed in the City of Rensselaer's offices, even where pets are generally prohibited.

Anyone who requires an auxiliary aid or service for effective communication, or a modification of policies or procedures to participate in a program, service, or activity of the City of Rensselaer, should contact the ADA Compliance Officer at (518) 462-4839 as soon as possible but preferably no later than 48 hours before the scheduled event. Requests later than 48 hours will be accommodated to the extent possible.

The ADA does not require the City of Rensselaer to take any action that would fundamentally alter the nature of its programs or services or impose an undue financial or administrative burden.

Complaints that a program, service, or activity of the City of Rensselaer is not accessible to persons with disabilities should be directed to the ADA Coordinator.

The City of Rensselaer will not place a surcharge on a particular individual with a disability or any group of individuals with disabilities to cover the cost of providing auxiliary aids/services or reasonable modifications of policy, such as retrieving items from locations that are open to the public but are not accessible to persons who use wheelchairs.

APPENDIX F: GRIEVANCE PROCEDURE

City of Rensselaer Grievance Procedure

Under the Americans with Disabilities Act

This Grievance Procedure is established to meet the requirements of the Americans with Disabilities Act of 1990 ("ADA"). It may be used by anyone who wishes to file a complaint alleging discrimination on the basis of disability in the provision of services, activities, programs, or benefits by the City of Rensselaer. The City of Rensselaer's Personnel Policy governs employment-related complaints of disability discrimination. In cases where the complaint is against one of the City of Rensselaer's sub-recipients of federal highway funds, the City of Rensselaer will assume jurisdiction. Complaints against the City of Rensselaer will be referred to the NYS Department of Transportation's Office of Civil Rights for proper disposition.

The complaint should be in writing and contain information about the alleged discrimination such as name, address, phone number of complainant and location, date, and description of the problem. Alternative means of filing complaints, such as personal interviews or a tape recording of the complaint, will be made available for persons with disabilities upon request.

The complaint should be submitted by the grievant and/or their designee as soon as possible but no later than 180 days from the last date of the alleged violation to:

Assistant Planning Director
City Hall, 62 Washington Street
Rensselaer, NY 12144
Phone: 518-462-4839

Within 60 days after the filing of the complaint, an investigation shall be conducted and completed, including, if necessary, a meeting with the complainant to discuss the complaint and possible resolutions. No later than 60 days after complaint submittal, a determination shall be issued by the coordinator, and where appropriate, in a format accessible to the complainant, such as large print, Braille, or audio tape. The determination will explain the position of the City of Rensselaer and offer options for substantive resolution of the complaint.

If the determination by the coordinator in writing does not satisfactorily resolve the issue, the complainant and/or his/her/their designee may appeal the decision within 30 calendar days after receipt of the response to the coordinator.

The coordinator in writing will review the complaint and, if necessary, meet with the complainant to discuss the complaint and possible resolutions. Within 60 calendar days

after receipt of the appeal, the coordinator in writing will respond and, where appropriate, in a format accessible to the complainant, with a final resolution of the complaint.

All written complaints received by the coordinator in writing, and responses from these two offices will be retained by the City of Rensselaer for at least three (3) years.

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APPENDIX G: ADA COORDINATOR CONTACT INFORMATION

City of Rensselaer ADA Coordinator

All complaints and/or questions regarding accessibility of any public buildings, pedestrian facilities within the right-of-way, or other programs, services, or activities of the City of Rensselaer should be directed to designated official below.

Title: Assistant Planning Director

Telephone: (518) 462-4839

Mailing Address:

City Hall
62 Washington Street
Rensselaer, NY 12133

APPENDIX H: PUBLIC COMMENT

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